



Town of Brookhaven Long Island



2004 Montauk Highway
Corridor Study & Land Use
Plan for Mastic & Shirley

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ACKNOWLEDGEMENTS

Graphics and design standards originated from

Traditional Neighborhood Design Manual

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I. INTRODUCTION

The Brookhaven Town Board enacted a moratorium affecting new land use applications within the 1.6 mile Mastic/Shirley Montauk Highway Corridor Study Area at the August 6, 2002 public hearing. The Study Area extends approximately 200 feet north and south of Montauk Highway from the William Floyd Parkway to the Forge River.

The moratorium was adopted in order to identify and address present and future development needs within the Study Area in response to community concerns of disorganized and inappropriate development exacerbating existing traffic conditions. The consensus of the community was summarized in The Montauk Highway Project, a document that echoed many of the issues identified in the Town's 1996 Land Use Plan.

These issues included commercial sprawl, traffic congestion, and general quality of life in the area. Due to continued commercial development pressure and Suffolk County traffic congestion mitigation plans, the Town recognized the need to expeditiously review land uses and available governmental authority to further the community's and the Town's development goals.



An imminent Federal Transportation Project for the reconstruction of Montauk Highway also influenced the timing of the moratorium. The Federal Transportation Project provides a unique and timely opportunity to fund significant changes to the existing infrastructure and improve and enhance the character of the Study Area.

The Montauk Highway Corridor currently lacks a destination identity or sense of place. The area can be difficult to safely navigate and inconvenient for residents seeking goods and services. People in nearby communities are not encouraged to support the existing commercial properties. Development has not been contained in a compact pattern that could have increased utilization while fostering a greater sense of community. The lack of defined areas where a number of different goods and services can be obtained makes it difficult to run errands without multiple trips.

There is complete lack of architectural cohesion within the Study Area where small frame residences are interspersed with offices, automobile related uses, strip retail centers and chain stores. Although the Study Area straddles the two hamlets of Mastic and Shirley, there is no distinction along Montauk Highway separating the hamlets.

Review and compilation of the 1996 Land Use Plan, and the community's visioning document in 2002, reveal commercial sprawl as the most significant deterrent to achieving the quality of life most residents desired. The Town's power of zoning appears to be the most applicable governmental authority available to remedy inappropriate development and associated congestion and pedestrian alienation within the community.

The community envisioned three neighborhood centers separated by a mix of residences or innocuous commercial or office uses within residentially designed buildings. The three neighborhood centers are often referred to as traditional downtown neighborhoods with a mix of retail, service, office and high density residential. This type of community is realized within many hamlets of Long Island, particularly along the Montauk Highway Corridor, in such communities as Sayville, Patchogue, Bellport, Center Moriches, Westhampton and Southampton.



A Traditional Neighborhood includes a mixture of housing choices (large and small), outbuildings, small apartment buildings, shops, offices, public services and civic buildings, and other nearby employment opportunities interwoven within the neighborhood, all within close proximity. Buildings are diverse in function, but compatible in size and character. A variety of housing types, including single-family residential, duplex, townhomes, condominiums, and apartments, are mixed throughout the neighborhood to meet the needs of diverse residents with varied ages and incomes.

The advantages of the traditional neighborhood, i.e., the type that had been prevalent in America prior to WWII, lie primarily in their ability to remain economically

viable during periods of economic hardship. The sustained economic viability manifests as a well attended, well maintained, safer and higher end development. The mixed residential and commercial uses complement and support each other relative to the marketplace. The higher density residential component remains available to support local service and retail and in complement, a work force remains available to power the retail and service components.

A Main Street District will serve as a focal point of a Traditional Neighborhood Development containing retail, commercial, civic, and public services to meet the daily needs



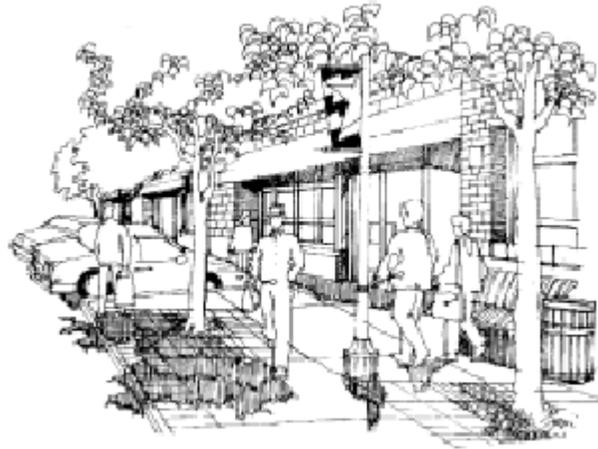
of community residents. A Main Street District is pedestrian oriented and is designed to encourage pedestrian movement between a mixed residential component and a Main Street District. Main Street District uses include small-scale retail shops, restaurants, offices, banks, hotels, post office, governmental offices, churches, community centers, and attached residential dwellings.

The transportation network should be equitable for both pedestrians and vehicles, and designed to accommodate all modes of transportation. A Traditional Neighborhood consists of a network of interconnecting streets and blocks with respect for the natural landscape. A variety of street designs are laid out in a grid-like network with smaller blocks to provide alternate routes to every destination. This layout diffuses automobile traffic and shortens walking distances. This permits most streets to be narrower with slower traffic, as well as having on-street parking, trees, sidewalks and buildings close by with increased pedestrian activity.

As the retail and service components are supported, they in turn provide a better residential atmosphere that is kept clean, remains convenient to markets, is well-lit vibrant and desirable. The contrast to the traditional neighborhood center with increased density at its center and a reduced residential density at its edges, is commercial sprawl. Commercial sprawl is unplanned, indiscriminate retail development without regard for traditional neighborhoods or adjoining uses or development.

Green spaces become fragmented and unsafe as they can provide a haven for undesirable uses. It results in an area without a defined identity and with conflicting and competing uses.

Well-configured public spaces such as squares, plazas, greens, landscaped streets, greenways, and parks are woven into the pattern of the Main Street District and dedicated to the social activity, recreation, and visual enjoyment of the residents of the Town of Brookhaven. The neighborhood design creates a hierarchy of useful open



spaces: a formal square in the Main Street District; parks and playgrounds throughout the Main Street District; and street environments designed to promote walking and casual meetings between residents.

The blight of commercial sprawl can generally be traced back to the underlying zoning in place that permitted the retail area to spread indiscriminately without cohesiveness until the only way a consumer could possibly fulfill basic consumer needs was to use the automobile. This was further exacerbated by the advent of the two-car (plus) family, which created the opportunity for retail to spread further away from pedestrian based communities.

Another significant force has been the development of “super” stores on major arterials that draw mobile consumers to the very perimeter of communities. These big box, mega-stores have the benefits of large corporate buying power, increase automobile exposure on major thoroughfares and provide a minimal investment in infrastructure, generally building disposable boxes that fail to make an aesthetic (or social) contribution to the community.

New jobs at the fringes of communities are disconnected from the places people live, generating more traffic. Poorly planned sprawl threatens the quality of life as the growing infrastructure leads to higher taxes, less open space, and the deterioration of the very qualities that people moved to these communities to enjoy.

This plan intends to provide a basis by which future development and redevelopment along the Montauk Highway Corridor and the peripheral residential areas can achieve a Traditional Neighborhood. Through land development techniques, appropriate siting of structures and parking, providing appropriate infrastructure and roadway improvements, Central Business Districts can be created and maintained, thereby establishing a true sense of place for these communities.

II. GOALS

These goals are developed as a guide to help shape the direction that future land use and planning analysis will take and serve to assist the Town of Brookhaven at all stages of the planning process within the Study area and surrounds.

- Develop defined Main Street Business Districts.
- Development of Main Street Districts that are controlled and compact, would create more traditional neighborhoods that can increase utilization of the district while fostering a greater sense of community.
- Provide for a mix of land use opportunities within the Main Street Business Districts.
- Design transitions between Main Street Business Districts.
- Create strong economic activity to provide jobs and adequate tax base.
- Develop appropriate zoning regulations to ensure proposed development in compliance with these goals.
- Support appropriate roadway improvements to adequately serve the adjacent land uses. Ensure that improved roadways are aesthetically pleasing.
- Introduce traffic calming measures to enhance routing of both local and through traffic.
- Provide Main Street Districts with convenient parking that will reduce vehicle usage, as residents will no longer need to drive from one store to the next.

- Create pedestrian-friendly Main Street Districts with attractive landscaping and public parks and plazas.
- Provide affordable housing opportunities within the Main Street District.
- Develop incentive techniques to accomplish these goals.
- Enforce existing codes, ordinances and regulations.
- Eliminate deterioration and obsolescence.
- Provide for larger and centrally located public spaces to provide a staging area for community events and festivals that increase civic identity and encourage participation.
- Enhance the aesthetic appeal of the area with particular attention to the streetscape and building architecture.
- Improve overall streetscape by moving utilities underground and increasing peripheral landscaping to screen gray areas such as parking lots.
- Establish architectural cohesion within the community to promote a sense of place and identity.

III. VISION

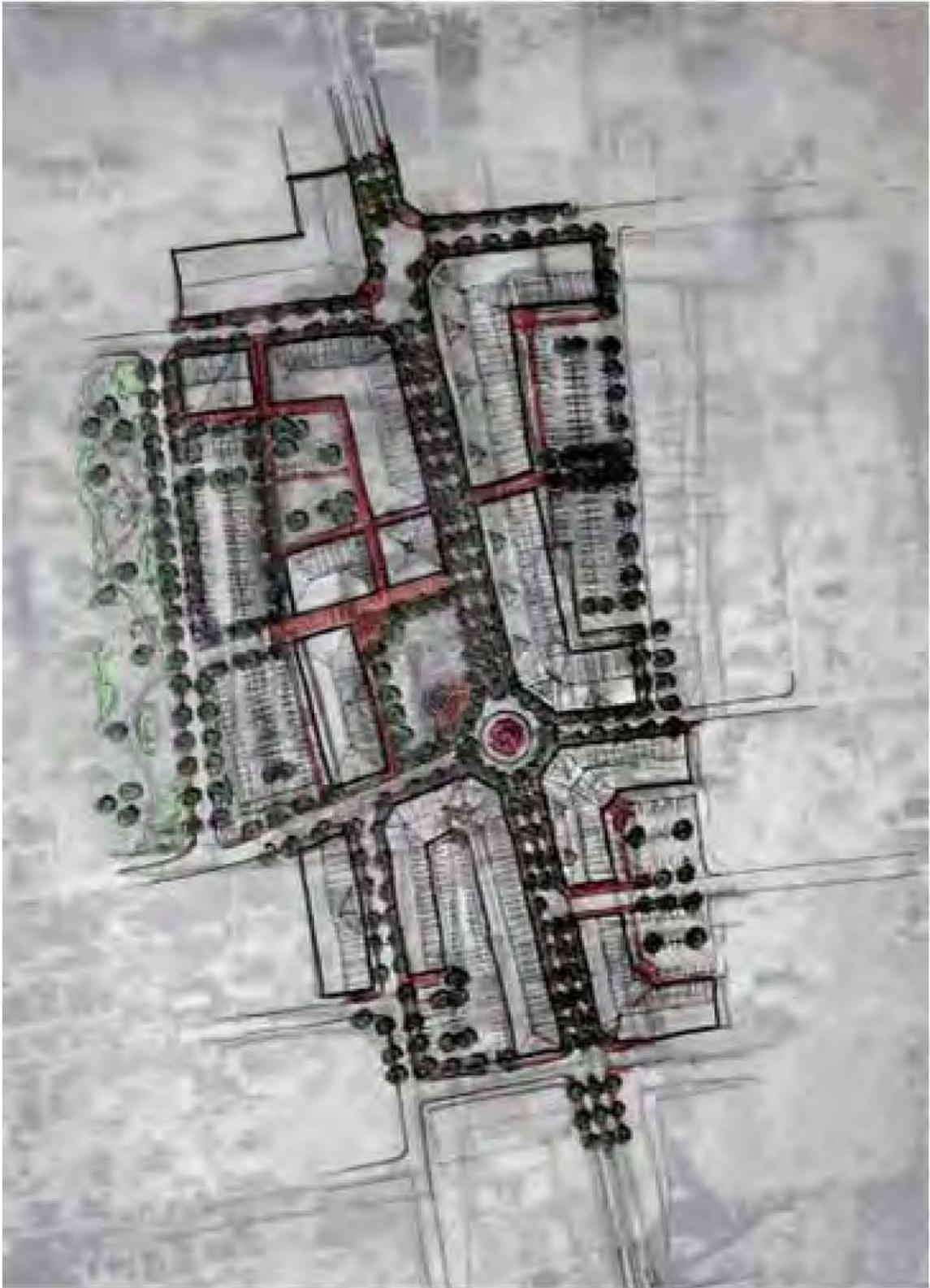
In early 2002, the Mastic/Shirley residents took part in a series of walking audits, workshops, training sessions, design studios and public presentations. The work focused on developing town streetscape plans for Montauk Highway, including both the street itself and the land bordering it on either side. The visioning report summarizes the work of the more than 500 community residents who participated in the visioning process.

The vision report, dated April 2002, identifies the problems the community faces with the existing conditions of the Montauk Highway Corridor, the principles by which the community can achieve its goals, and the solutions recommended for the overall corridor.

The vision report shall be incorporated by reference into this study.



Shirley Vision Figure 01



Mastic West Vision Figure 02



Mastic East Vision Figure 03

IV. LAND USE AND ZONING

1. Existing Conditions

A. Commercial/Industrial

The commercialization of the Montauk Highway Corridor between Shirley and Mastic began in 1937 with the adoption of the Code of the Town Of Brookhaven and the “Strip Zoning” of this Montauk Highway Corridor with 100 feet of commercial zoning on either side of the corridor. Over the course of years many lands have been granted extensions of the commercial J-2 Business zoning. Within this area along both the north and south sides of the Montauk Highway corridor an uneven mix of commercial land use and zoning patterns has emerged. In many cases the existing residential dwellings fronting on this corridor were converted to a commercial uses. Some single-family residential land uses continue to exist today.



Main Street Montauk Highway, West Hampton, NY

Unlike the neighboring communities to the east and west, a downtown or main street business district never developed along this corridor. The exception is found at the very western end of the corridor, wherein in the 1950's several of the structures were situated along the roadways at Montauk Highway and Herkimer

Street in such a way as to attempt to establish itself as a central business district.

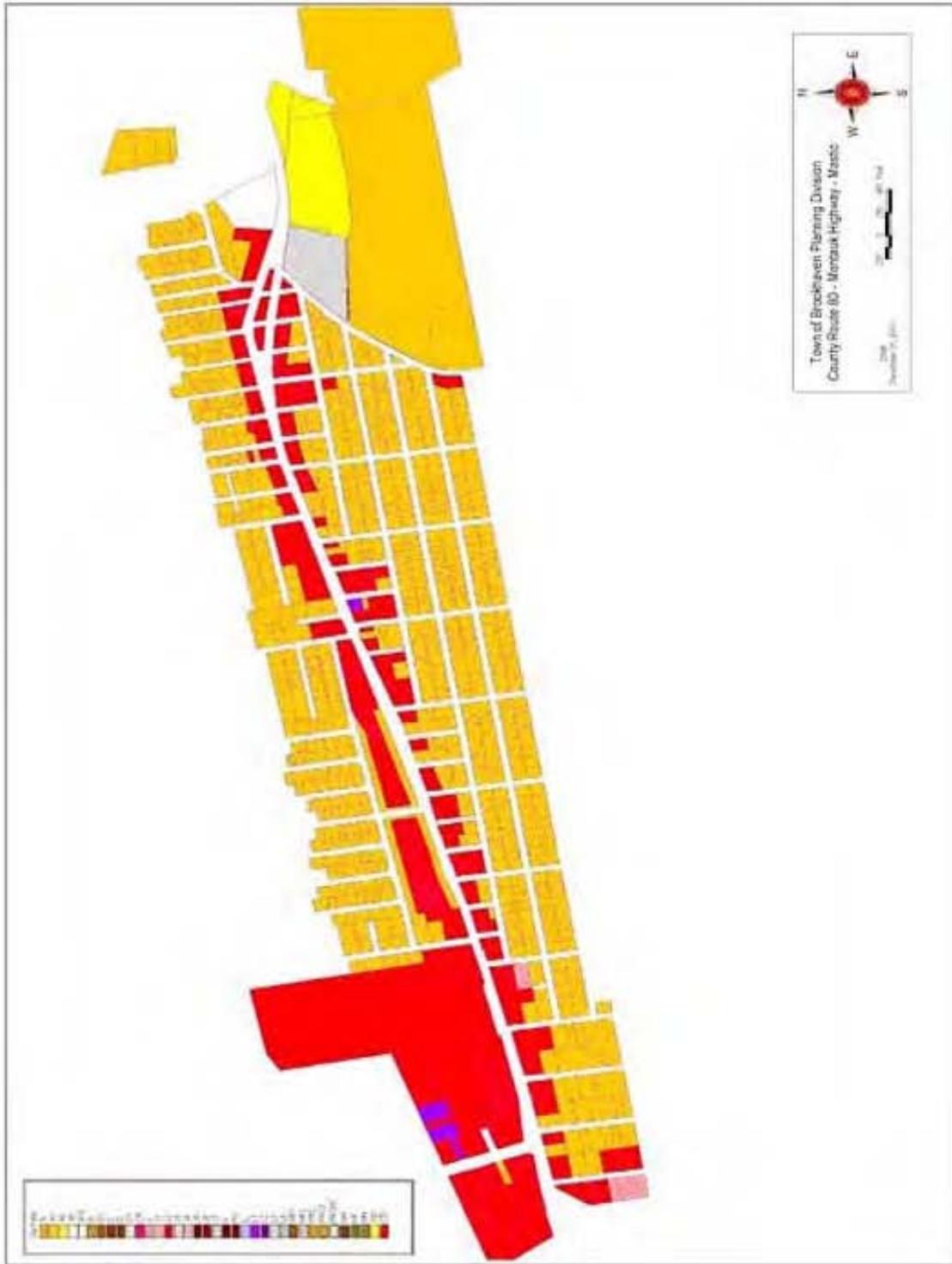
The visual images along the Montauk Highway Corridor are poor. Very few or no street trees, little or no on-site vegetation or landscaping, excessive and poorly located signage, poor or perhaps no off-street parking areas and very few areas with curbs and sidewalks. These combine to make this corridor unfriendly and even unsafe for pedestrian activity. A common architectural theme along the

corridor cannot be found which contributes to the poor visual quality of the corridor.

Field analysis found two community shopping centers. One is located on the north side of Montauk Highway just west of Park Avenue. This site is presently zoned J-2 Business and occupied with almost 300,000 square feet of retail space within 30 storefronts, with associated parking and landscaping. Three major



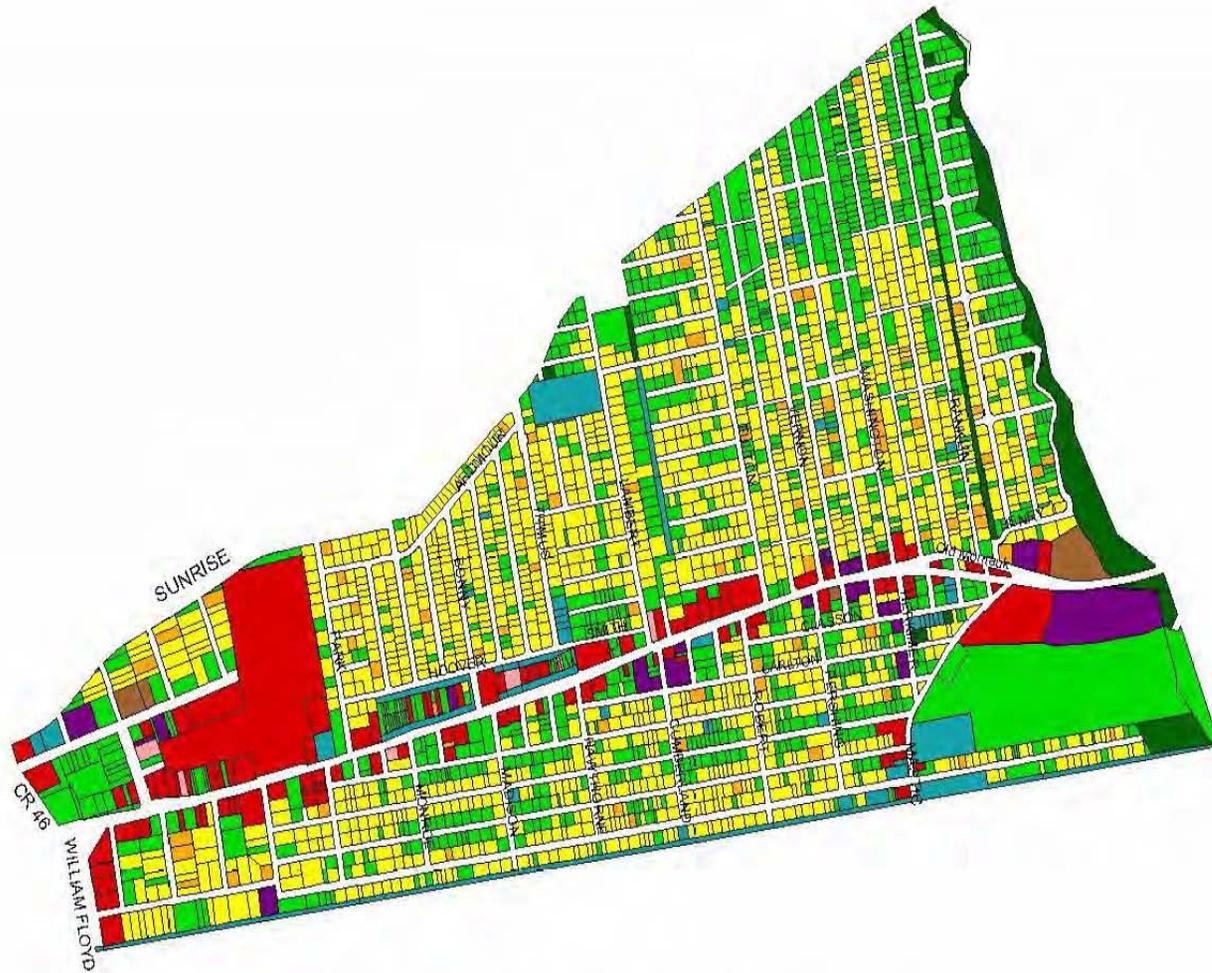
Southport Shopping Center



Existing Zoning Figure 04



2001 Aerial Figure 05



- StcI_town
Mastic clip 1.shp
- Open
 - Vac
 - Ag
 - Med Res
 - Med High Res
 - High Res
 - Office
 - Comm
 - Ind
 - Inst

Montauk Highway Corridor Existing Land Use for Mastic & Shirley

JS January 2004

900 0 900 Feet

tenant anchors occupy this site, including Kohl's and Sears Hardware, the third is a supermarket and another is presently under construction.

Unlike other traditional community shopping centers throughout the Town of Brookhaven, this center could be classified as a regional shopping center. The majority of the storefronts within this center are primarily national retailers. These include Bath & Body Works, Kay Bee Toys, Sleepy's, Children's Place and The Gap. Typically these types of national retailers are found in regional shopping centers or malls.

The other community shopping center is located at the southeast corner of Montauk Highway and Mastic Beach Road. This site is also zoned J-2 Business and is occupied with 43,000 square feet of retail space within 12 storefronts, with associated



Movieland at Mastic

parking and landscaping. One major tenant anchor occupies this site, which is presently a (7 screen/900 seat) multiplex movie theater. A detached bank with a drive thru also occupies space within this center. The remaining storefronts within this center are of a general retail, physical therapy or food service nature.

The vast majority of the remaining parcels of land fronting along the north and south sides of Montauk Highway are zoned for J-2 Business purposes. Eight strip shopping centers were identified with approximately 59 storefronts. The remaining retail establishments are of a single retail general user type.

Six fast food restaurants were identified, primarily along William Floyd Parkway, two along Montauk Highway in the western part of the corridor. Two sit down restaurant/bar establishments were also found, one of which is presently vacant.

Office uses along the Montauk Highway Corridor mostly consist of small individual single use buildings with a storefront like office uses. Most office uses in this locale are semi-retail such as car insurance, and real estate office uses. Several sites



Two Story Office Building

were found to have a retail first floor use with a second story office or residential use. Two traditional office buildings were identified with approximately 10 leaseable office spaces (actual leaseable square footage is not known). One of these office buildings, along Grand Avenue, was found to be completely vacant, thereby contributing to the high office vacancy rate.



Mason Supply

Quite a few heavy commercial or industrial type uses were found. These types of uses include auto repair and service, auto body repair, tire sales, car stereo installation facilities, outdoor storage yards, plumbing supply warehouses, lumber yard, propane gas distributor, car washes and gasoline filling stations (two with

convenience stores). Two car wash establishments and five gasoline filling stations were identified.

Nineteen sites were identified with an auto related repair or maintenance use. Primarily these uses are not attractive land uses. The majority of these uses have no amenities such as landscaping, curb and sidewalks and improved parking

areas. Most have depot and outdoor storage areas and bay doors that are visible from the Montauk Highway Corridor.

Mixed in with all of those uses are other institutional quasi-public land uses such as churches, post office and community centers.

i. Inventory.

A complete inventory of all lands along the Montauk Highway Corridor was examined. Analysis consisted of present land use, accessory uses, number of tenants and condition of the site. The complete inventory has been attached as an addendum.

An analysis of building square footage was also performed. Based on information obtained from the Town of Brookhaven assessment records building square footage information has been tabulated by land use and attached as an addendum.

ii. Vacancy Analysis.

Review of all retail related land uses along the Montauk Highway Corridor found approximately 170 stores of which approximately 22 were vacant. This represents a 13% vacancy rate along this corridor.



Vacant Restaurant

This is significantly lower than the 21.5% commercial vacancy rate Town Wide, identified in the Town Of Brookhaven, 1996 Comprehensive Land Use Plan.

Alternatively, review of the existing office related uses, 31 office uses were identified. Of these 9 were found to be vacant, representing a 30% vacancy rate for office uses along the Montauk Highway Corridor.

Existing building square footages were calculated by using the Town Assessment information. It was found that approximately 170,000 square feet of general retail space presently exists, 315,000 square feet of commercial center space, 41,000 square feet of office space, 15,500 square feet of fast food restaurants, 10,000 square feet of gasoline filling stations, 67,000 square feet of industrial uses, 21,000 square feet of residential land uses, 5,000 square feet of mixed use and 3,800 square feet of civic uses. A table of this square footage land use breakdown is attached as an addendum.

B. Residential

Just outside of the Montauk Highway corridor frontage are residential land uses within an A-1 Residential zoning district. The residential development within this area is predominantly single family dwelling units on smaller lots, an average lot size of 10,000 square feet or smaller. This area is also known to widely use accessory apartments, many without proper Town permits or approvals.

A small multifamily development exists along the north side of Montauk



Planned Retirement Community

Highway and the west side of the Forge River, Consisting of rental dwelling units. This site is presently zoned for PRC-3 Residence, permitting occupants of 55 years or older only. One other multifamily land use was identified located at the southwest corner of Montauk Highway and Denton Place. The three-story, ten unit apartment

building, zoned J-2 Business District, was built in 1946.

One historic house, the Pettit (also know as the Petty House) House circa between 1797 and 1836 is located along the north side of Montauk Highway just west of Park Avenue. The significance of this structure is vital. Many historical residences exist within this historic community, such as the William Floyd Estate and the Manor of Saint George, this is a rare survivor of the six houses that were illustrated on the 1836 U.S. Coast Survey along Montauk Highway. However this is the last remaining historic house on the old Montauk Highway in the Shirley Mastic area. Although modest in scale and architectural detail, it is representative of a residential building type, virtually eliminated along this corridor, that represents the early residential style of the Mastic-Shirley area. Preservation of this house is therefore essential. This house is presently being restored for future office use.

There is one other structure was found on the Town of Brookhaven's Historic Building Structure Inventory. The Shore Manor restaurant built in the late 1920's.



Manor House Restaurant

C. Parks and Open Space

Within this study area there were no public spaces or parks. Most of the recreational and park activities within the locale are to the south along the Great South Bay or along the Forge River. Existing Civic uses include the Mastic Post Office and the Mastic Volunteer Ambulance Department.

The Town of Brookhaven has a Recreation Center located on Herkimer Street and Classon Avenue. This Recreation center provides for senior recreational

activates, adult and child day care and other such activities for all Town Of Brookhaven residents.

A small triangular shaped parcel of land owned by the Town Of Brookhaven welcomes travelers to the community at the William Floyd Parkway and Montauk Highway intersection. At this site is a welcome sign, flag pole and monument to veterans.



Town of Brookhaven Mastic Recreation Center

Another common area was found at the Montauk Highway and Old Montauk Highway intersection. This small area has a flag pole and veterans memorial area.

V. INFRASTRUCTURE

1. Existing Transportation.



Montauk Highway East from Prospect Place

Existing traffic patterns from the Mastic-Shirley peninsula are determined and limited by the existence of two Long Island Railroad at grade crossings that provide direct access to Montauk Highway and ultimately to Sunrise Highway. These crossings exist at William Floyd Parkway and Mastic Road, both being major

north-south arterials. Only William Floyd Parkway provides direct and convenient

access to the Sunrise Highway. Many of the secondary roads that connect these two major arterials are not entirely constructed, forcing all local and through traffic onto Montauk Highway to gain access to shopping or other major north/south routes. Shared parking fields and parking lot cross access designs are relatively non-existent causing shoppers to enter Montauk Highway to drive from one store to the next, thereby increasing vehicle congestion and adversely impacting traffic safety.

The overall streetscape presents as an unkempt array of freestanding signs, utility poles and overhead lines, dominated by parking lot gray fields. Street trees and other front yard landscaping enhancements are missing. In many areas, property maintenance requirements are not met. Underground utility work and roadway repairs have left patched and irregular pavement surfaces. In many cases the street pavement is contiguous to and without separation from the front yard parking pavement, due to a lack of curbs, landscaping and sidewalks throughout the corridor.

Five signalized intersections presently exist along this 1.6 mile long Montauk Highway corridor between William Floyd Parkway and Mastic Road providing needed traffic control at critical intersections. The first, located at William Floyd Parkway and Montauk Highway, controls all traffic movements along both



Montauk Highway East from Aletta Place

arterial roadways with a multiphase operation including dual turning lanes on critical approaches. The next, one-quarter mile to the east, is at Montauk Highway and Grand Avenue. This intersection provides an alternative route to and from the Southport Shopping Center and McGraw Street to the north, a parallel roadway to Montauk Highway. Another one quarter mile to the east is the signalized intersection of Montauk Highway and Southport Shopping Center / Garden Place. This intersection experiences significant congestion and delays at peak hours due to traffic volumes and limited capacity. The intersection of Montauk Highway and Titmus Avenue / Hawthorne Street

is located one half mile to the east of Garden Place. This intersection is significant because it provides a northerly route, Titmus Avenue, with a bridge over Sunrise Highway with direct access to Victory Avenue, which functions as the north service road of Sunrise Highway. However, Hawthorne Street terminates at the Long Island Railroad tracks to the south, without the benefit of a crossing. Finally, another one-half mile east of Titmus Avenue, is the intersection of Montauk Highway and Herkimer Street / Washington Avenue. Herkimer Street is a short connector to Mastic Road, which crosses the Long Island Railroad tracks at grade and further provides major north / south access to the Mastic-Shirley peninsula, including the hamlet of Mastic Beach to the south.

Suffolk County Transit provides public transportation with an existing bus system that provides limited service opportunities. Bus stop waiting areas are poorly marked and generally unprotected from the weather increasing low ridership trends. The apparently random bus stops within the Montauk Highway corridor exacerbate traffic congestion due to a lack of sufficient roadway shoulders as vehicles cross the roadway centerline to pass a bus slowing upon approach to a bus stop.

The Long Island Railroad operates a train station in Shirley along the southside of the tracks on the eastside of William Floyd Parkway. The station appears to be underutilized and has survived many system service cuts, but remains disassociated from the central commerce areas of Montauk Highway.

Pedestrian usage of the Montauk Highway Corridor is presently not supported, nor accessible for all residents, including those with physical challenges. The lack of continuous sidewalk areas and the state of disrepair of those existing segments create an unfriendly and unsafe walking environment. Street lighting within the corridor is limited and offers little comfort for pedestrians and creates an undesirable environment at night. There is a lack of pedestrian controlled crosswalks for



Pedestrian Walkway

pedestrians wishing to cross Montauk Highway and other busy cross streets. Existing crosswalks are not convenient or readily visible. There are no existing pedestrian-friendly neighborhood shopping centers with attractive landscaping and public places. Walking is neither a pleasant, nor a convenient way to travel within the area. Public spaces for community events are non-existent

The Montauk Highway corridor does not include a bicycle component as part of the transport system. It is currently difficult to safely and conveniently navigate a bicycle through the Corridor / Study Area. The lack of bike lanes creates a driving hazard when cyclists are forced to share the road with motor vehicles. There are no destinations that provide safe and convenient bike racks for cyclists. The lack of pedestrian and bicycle accommodations contribute to further traffic congestion, as residents are encouraged to drive rather than walk or bike.

2. Existing Other Infrastructure.

There has historically been no significant commitment to public infrastructure within the Study Area. The area is not sewered, all buildings utilize individual septic systems. Utility lines are above ground, and existing poles are prolific, random and placed within the right of way on both sides of the corridor. There has been no investment toward municipal parking areas. Presently there are no other improved infrastructures. Streetlights are not uniformly designed

VI. RECOMMENDATIONS

1. Land Use and Zoning.

A. Commercial.

In order to make this Mastic/Shirley Montauk Highway Corridor a viable and active community with a unique personality and create a sense of place, the development and redevelopment within the corridor must be done in a cohesive and coordinated manner. These recommendations will provide the foundation

and tools with which this corridor can grow and prosper while providing a visually appealing and pedestrian friendly community.

Three separate Main Street Districts will be established along Montauk Highway. Supplementing and supporting these Main Street Districts will be a mix of multi-family, two-family and single family zoning districts and land uses.

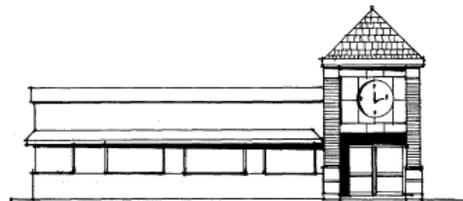
In order to separate the three Main Street Business Districts and draw a distinction between them requires the use of transitional lands and zoning as either single-family residential or low intense commercial uses, such as office uses consistent with a “J” Business District Zoning District. “J” Business District allows and permits such uses as single and two-family residential dwellings, office uses, mixed use (excluding retail), churches or similar worship, libraries or municipal buildings. All these uses within the transitional “J” Business areas will have a residential appearance, architecture, scale and setbacks. Parks and civic uses can also be located within the transition areas.

New Town Code provisions for Main Street District and the recommendations contained herein will establish the development criteria and standards necessary to accomplish the appropriate improvements. Many of the existing business and land uses may have to be relocated, discontinued or acquired for redevelopment.

Integrated into each Main Street District will be parks and civic uses. Roadway and infrastructure improvements will be provided consistent with a Main Street setting. These infrastructure improvements are an essential part of the overall redevelopment of this corridor.

Parking lots will be located behind or along the side of buildings. When located along the side of a structure, the parking lot will be screened with landscaping or with streetwalls.

Buildings on corner lots, at the intersection of major streets, or at the entrance to the Main Street District will be considered significant structures. Such buildings should incorporate special



architectural embellishments, such as corner towers, clock towers, cupolas, spires, or other similar features to emphasize their location and importance.

The development and redevelopment of this corridor will establish a Traditional Neighborhood Development or Main Street District.

i. Main Street Districts.

The western or Shirley Main Street District will begin along Montauk Highway from William Floyd Parkway and extend to the Park Place intersection. This will also include the lands along Grand Avenue and McGraw Street. The central or West Main Street District will be located along Montauk Highway beginning from the east side of Bonnie Drive/Madison Street and extend to the west side of Oakland Avenue/Berkley Place. The eastern or East Main Street District will begin along Montauk Highway at the Denton Place/Stuyvesant Avenue intersection and extend to the western side of the Forge River. This area will also include some lands along Herkimer Street and along Old Montauk Highway.

(a) Shirley Main Street District - Montauk Highway, McGraw Street and Grand Avenue.

An opportunity exists to create a new Shirley Main Street District along Montauk Highway with a focus at the Grand Avenue/Montauk Highway intersection and the intersection of Grand Avenue and McGraw Street, see the proposed Land Use Plan, Figure 9. This district would be consistent with the community's vision report.



The areas designated for the Shirley Main Street District are as follows:

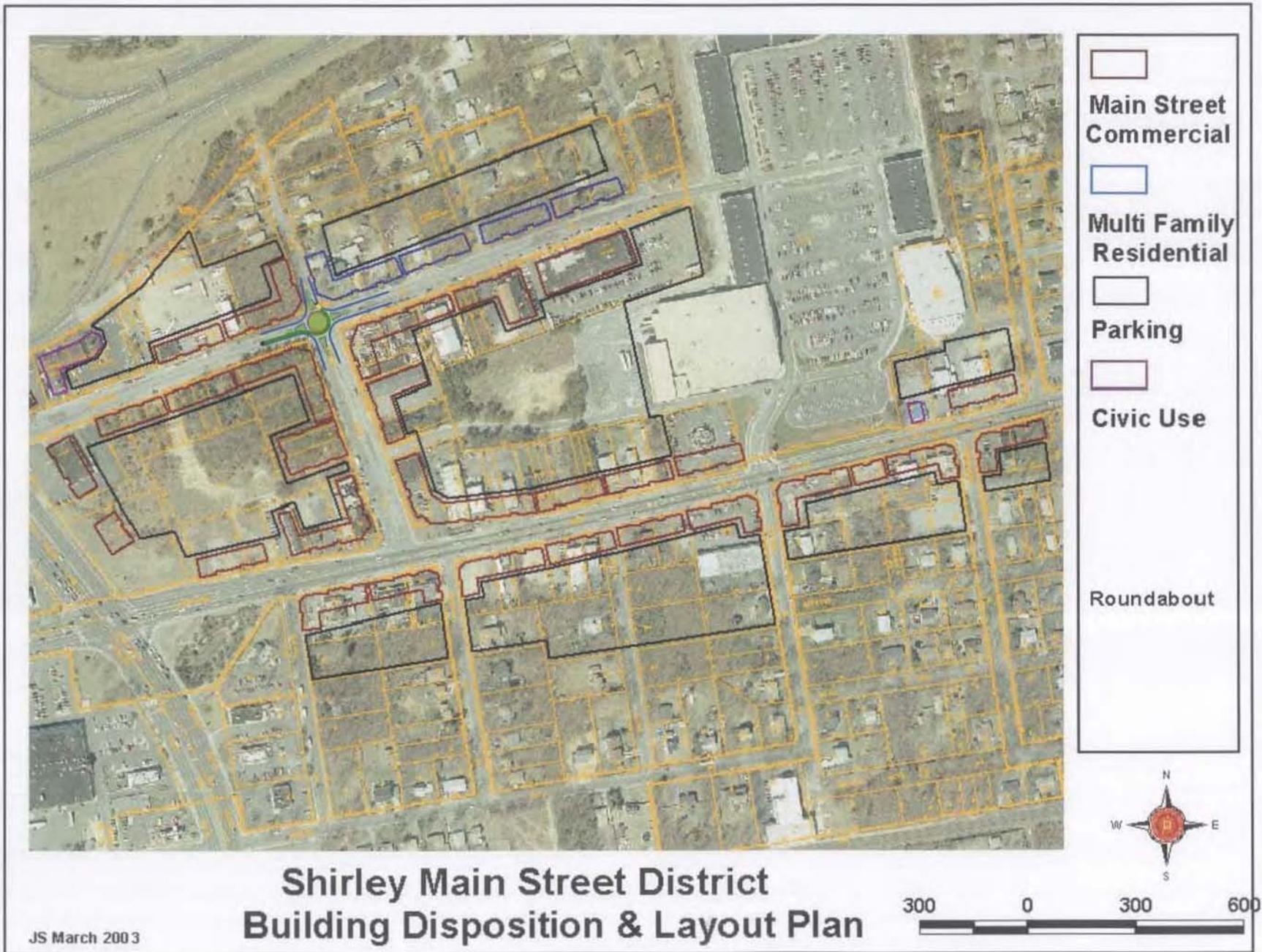
- William Floyd Parkway and McGraw Street intersection and running along William Floyd Parkway.
- North and south sides of McGraw Street and continuing along the east and west sides of Grand Avenue.
- North side of Montauk Highway from William Floyd Parkway to the entrance to the Southport Shopping Center.
- North side of Montauk Highway from Southport entrance to Park Avenue.
- South side of Montauk Highway from Ormond Place to Garden Place and continuing to Park Place.



The Shirley Main Street District will be primarily composed of a first floor commercial (or other similar use in compliance with the new Town Code for Main Street District (J-6 Business)) with a second (or third) story residential or office use. Space is available for several larger box uses, however pursuant to the new Town Code for the Main Street District (J-6 Bus.), no single use shall exceed 60,000 square feet in building size.

The layout and disposition of the buildings along all these roadways will be situated along the roadway frontages with all of the parking in the rear of the buildings or within public parking areas. Alleyways shall be provided for pedestrian access from the parking facilities to the street frontage. A typical building layout and disposition plan is shown in Figure 6.

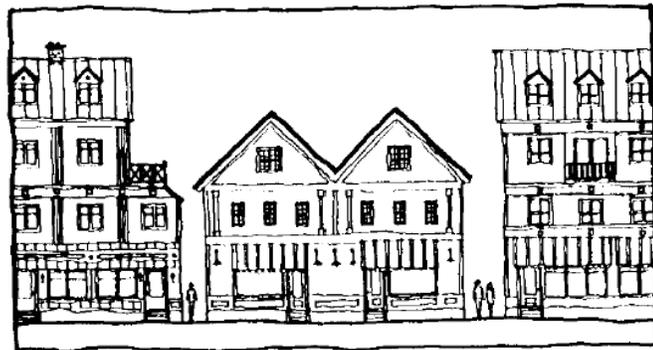
Figure 06



The lands along the north side of McGraw Street from Grand Avenue east to the lands of the South Port Shopping Center (approximately two hundred foot deep lots) should be merged and developed as a Multi-Family Residential development. Building location, scale and architecture shall be similarly designed as those buildings in this Main Street District and placed along the McGraw Street with the parking to the rear of the lots/buildings.

For the lands along William Floyd Parkway, due to the high intensity of traffic along William Floyd Parkway, the present 70 foot building setback would be more appropriate.

However the existing front yard parking should be relocated to the rear yard and combined with other proposed



parking areas. The front yards should be landscaped to provide a visually pleasing appearance from this major roadway.

Southerly access from Southport to Grand Avenue will be utilized for internal parking lot access and new street frontage to parcels presently fronting on Montauk.

Public space, including civic spaces, private plazas and public parks, should be intermixed throughout the Main Street District.

Presently the Town has several proposed site plan applications within this area. The proposed CVS at the intersection of Montauk Highway and Garden Place has been designed the site in accordance with the recommendations and development standards contained herein and in accordance with the proposed Town Code for the Main Street District. The other proposed site plan is for Floyd Harbor Shopping Center located

at the southwest corner of McGraw Street and Grand Avenue. This applicant has worked with staff to design the site in accordance with the community vision and town code provisions for the proposed Main Street District.

(b) West Main Street - Titmus Avenue & Montauk Highway.

A new West Main Street District along Montauk Highway should be created with a focus at the Titmus Drive/Washington Avenue intersection, see the proposed Land Use Plan, Figure 9. This district would be consistent with the community's vision report.

The areas designated for the West Main Street District are as follows:

- North Side of Montauk Highway from Bonnie Drive to the west side of Oakland Avenue.
- South Side of Montauk Highway from Madison Avenue to the west side of Berkley Place.

The West Main Street District will be primarily composed of a first floor commercial (or other similar use in compliance with the new Town Code for Main Street District) component with a second (or third) story residential or office use.

The disposition of the buildings along these roadways will be situated along the Montauk Highway frontage with all of the parking in the rear of the buildings or within public parking areas. Alleyways shall be provided for pedestrian access from the parking facilities to the street frontage. A typical building layout and disposition plan is shown in Figures 7.

Public space, including civic spaces, private plazas and public parks, should be intermixed throughout the main street district.



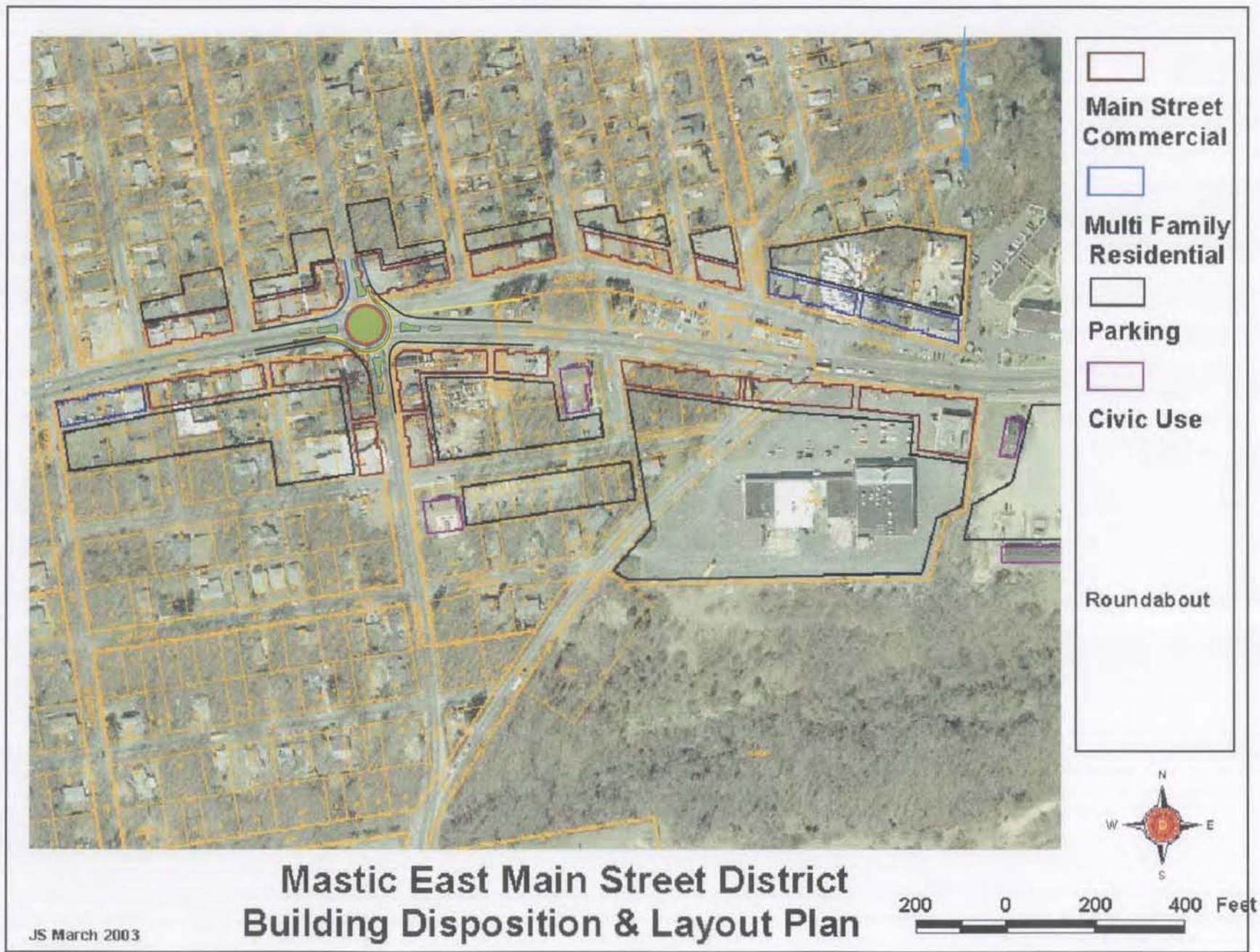
-  Main Street Commercial
-  Multi Family Residential
-  Parking
-  Civic Use
-  Roundabout



Mastic West Main Street District Building Disposition & Layout Plan

JS March 2003

Figure 08



(c) East Main Street - Herkimer Street & Montauk Highway.

The East Main Street District along Montauk Highway will focus at the Herkimer Street intersection will be a revitalization or redevelopment of an old main street district, see the proposed Land Use Plan, Figure 9. Several of the older structures in this area were located and situated in a manner consistent with a traditional neighborhood development or business district.

The building locations in this area were constructed along the roadway with parking in the rear or sides of the structures. Some of the existing uses are also consistent with a Main Street District, with a first story commercial or office use and a second story or accessory residential use.

However over its long history, the existing infrastructure such as curb and sidewalks has fallen into disrepair. This area also lacks the amenities, such as street trees and furniture, and lacks an overall architectural cohesion.

The areas designated for the East Main Street District are as follows:

- South Side of Montauk Highway from Pershing Street to the western side of the Forge River.
- North Side of Montauk Highway from Stuyvesant Avenue and continuing along the north side of Old Montauk Highway to Henry Street.
- Just north of the Montauk Highway intersection with Washington Avenue and extending along both sides of Herkimer Street to Carlton Avenue.

The East Main Street District will be primarily composed of a first floor commercial (or other similar use in compliance with the new Town Code for Main Street District) component with a second (or third) story residential or office use. Space is available for several larger box uses, with the redevelopment of the existing shopping center, however pursuant

to the new Town Code for Main Street Business District, no single use shall exceed 60,000 square feet in building size.

The location of the buildings along all these roadways will be along the roadway frontages with all of the parking in the rear of the buildings or within public parking areas. Alleyways shall be provided for pedestrian access from the parking facilities to the street frontage. A typical building layout and disposition plan is shown in Figure 8.

The lands along the north side of Old Montauk Highway from the existing PRC site known as Lakeside to Henry Street should be merged and redeveloped as a Multi-Family Residential development, pursuant to the Main Street District Code, as a continuation of the existing multifamily to the east. Building location, scale and architecture shall be similarly designed as those buildings in this Main Street District and placed along the Old Montauk Highway with the parking to the rear of the lots.

Another Multi-Family Residential land use could also be considered along Montauk Highway between Pershing Street and Denton Street. This site is presently occupied with a three story multifamily use and an open air car lot. The entire site could be redeveloped as a cohesive multifamily development. Building location, scale and architecture shall be similarly designed as those buildings in this main street district and placed along Montauk Highway with the parking to the rear of the lots.



A couple of civic uses are present in this district. One is the Mastic Volunteer Ambulance Company located along Montauk Highway and Gunther Place. This use is designed with the building along Montauk

Highway with the emergency vehicle access on Montauk Highway and the remaining parking and entrance on Gunther Place. The other civic use is the Town Of Brookhaven Mastic Recreation Center located along Herkimer Street and Classon Avenue. This site is situated with the building along Herkimer Street with the parking along Classon Avenue. These two uses are important parts of the new Main Street District.

The island between Montauk Highway and Old Montauk Highway from Etna Place, running west to it's closed intersection with Montauk Highway should be acquired and utilized for a civic use such as public common area or town square.

This main street district also provides a unique opportunity to create a public park and large civic area. The lands along the Forge River offer an opportunity to provide public access for park and civic areas while providing development opportunities compatible with J-6 Standards. See the proposed Land Use Plan, Figure 9.

(d) Transition Areas.

The areas between the three Main Street Districts shall be designated as transitional areas to separate the Main Street Districts and establish a green space along the Montauk Highway road frontage, see the proposed Land Use Plan, Figure 9. This is also consistent with the community visioning. The land uses that could be established in this area can be either single-family residential or low intense commercial uses, such as office uses consistent with a "J" Business District Zoning District. "J" Business District allows and permits such uses as single and two-family residential dwellings, office uses, mixed use (excluding retail), churches or similar worship, libraries or municipal buildings. All these uses within the transitional "J" Business areas will have a residential appearance, architecture, scale and setbacks. Parks and civic uses can also be located within the transition areas.

Customary home occupations (a maximum of 25% of the first floor area can be used for office and other home commercial uses) or limited use commercial and offices could also be a part of this transitional district. However all uses within this district must have a residential appearance, scale and architecture. Front yard parking will not be permitted with the exception of driveway access for the single-family homes. The lands along Montauk Highway from Park Avenue to just east of Bonny Drive/Madison Street should be developed pursuant to the “J” Business District. Building location, scale and architecture shall be designed as typical single family dwellings, with typical setbacks and no parking within the front yard.

The lands along Montauk Highway from Oakland Avenue/Berkley Place Pershing Street/Stuyvesant Avenue should be developed pursuant to the “J” Business District. Building location, scale and architecture shall be designed as typical single family dwellings, with typical setbacks and no parking within the front yard.

Park and open civic uses are encouraged in these transitional areas.

B. Residential.

The areas already committed to single family residential land uses should continue as such. However, zoning incentives could be offered to several of the areas, which will have a greater visual effect from the proposed Main Street. A zoning district



Residential Dwelling on Montauk Highway

that allows for legal two family residential developments, such as the “D”

Residence Zoning District could be considered. See the proposed Land Use Plan Figure 9.

An area that could be considered for an incentive residential zoning district, is the existing single family homes along Titmus Drive from Montauk Highway to Sunrise Highway. Due to the proposed improvements to Titmus Drive a “D” Residential Zoning District could be considered.

As previously stated several areas have been identified for some kind of multifamily development within the main street district. These types of development are important to help provide a steady presence of people within the main street district in order to support the business in these districts.

Another key residential component to a successful Main Street District is providing the opportunity for second (or third) story residential uses over the first floor commercial or office uses fronting on the Main Street. These residential dwelling units also provide for a much needed affordable housing market in this area.

All residential developments in selected areas shall incorporate the design and placement of street trees. However, a formal street tree planting scheme shall not be required if a landscape plan is approved for a development area.

Individual streets should be recognizable by the tree used. They may be planted with more than one species of tree to accent common areas and focal points within the development. Different streets should be planted with different species of trees to provide interest and to ensure disease and pest resistance throughout a development and the Town.

As much as possible, developments should include a mix of housing styles such as townhomes, condominiums, garden apartments, duplexes, and single-family units to create a mixed-use community with housing options for residents of all ages and incomes.

When located adjacent to single-family dwellings, the design and appearance of multiple family dwellings must have similar massing, height, roof pitch, and architectural features – including front porches; cornice lines; horizontal lines of

windows; and architectural embellishments, such as: shutters, dormers, belvederes, chimneys, etc., to create the appearance of single-family dwellings.

i. Multi-Family Residential Uses.

Several areas have been identified for Multi-Family Residential land uses within the main street districts. The differences between the two land uses primarily consist of the layout and disposition of the buildings on the lots. Traditional Multi-Family land uses and zoning provide for certain setbacks from the roads and buffering from adjacent land uses.

The MF/PRC Residential developments within the Main Street District should provide for the same building layout and disposition on the site as the other buildings within the Main Street District. In order to accomplish this design, variances will be required by the Town Board from the MF/PRC Zoning Codes. The first and second (or even third) story of the MF/PRC building shall be residential.

The lands along the north side of McGraw Street from Grand Avenue east to the lands of the South Port Shopping Center (approximately two hundred foot deep lots) should be conjoined and developed as a MF/PRC Residential development within the main street district. Building location, scale and architecture shall be similarly designed as those buildings in this Main Street district and placed along the McGraw Street with the parking to the rear of the lots.

The lands along the north side of Old Montauk Highway from the existing PRC site known as Lakeside to Henry Street should be merged and redeveloped as a MF/PRC Residential development as a continuation of the existing multifamily to the east. Building location, scale and architecture shall be similarly designed as those buildings in this main street district and placed along the Old Montauk Highway with the parking to the rear of the lots.

Another MF/PRC Residential use should be considered along Montauk Highway between Pershing Street and Denton Street. This site is presently occupied with a tree story multifamily use and an open air car lot. The entire site can be redeveloped as a cohesive MF/PRC Residential development. Building location, scale and architecture shall be similarly designed as those buildings in this Main Street district and placed along Montauk Highway with the parking to the rear of the lots.

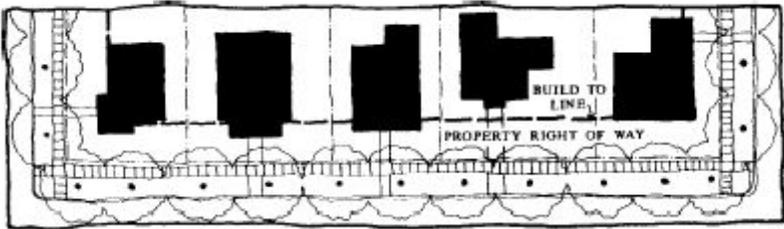


C. Development Standards and Guidelines.

The development standards and guidelines shall include those criterion that are set forth in the *Main Street Business District Design Manual* as referenced herein:

i. Building:

- Buildings should be designed in an attractive and interesting manner to define the image of the community.
- A minimum five foot building setback from front property line shall be required. This five foot area shall be utilized for planted landscaped areas or to provide for outdoor seating or display area.



- A consistent building line should be maintained at the setback line along the street. However, projections of porches, bay windows, stoops,

and their minor building masses into the building line are encouraged to create an interesting streetscape.

- Buildings on corner lots, at the intersection of major streets, or at the entrance to the Main Street District shall be considered significant structures. Such buildings should incorporate special architectural



embellishments, such as corner towers, clock towers, cupolas, spires, or other similar features to emphasize their location and importance.

- In general, buildings should relate in scale and proportion to other buildings in the area. However, buildings of different sizes can be made architecturally compatible through skillful design and careful orientation.

- Windows and projecting wall surfaces to break up larger wall surfaces establish visual interest and provide visibility of the street and other public spaces encouraging social interaction.



- All sides of the building should be equally attractive. Architectural details such as texture, pattern, color, and building form used on the front

facade should be incorporated on all visible building facades. However, such requirements shall not apply to any façade(s) facing service courts or other areas generally not visible to the public.

- Protective entry courts, common vestibules, covered breezeways, or enclosed stair halls to reduce the number of visible doors, unless designed in a row house or townhouse manner oriented toward the street.

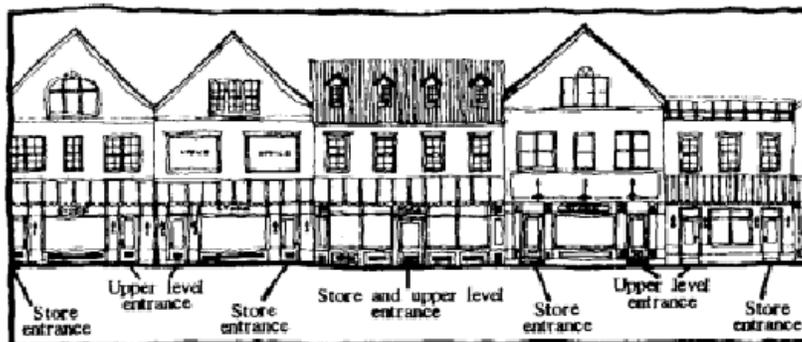
- Buildings shall avoid long, monotonous, uninterrupted walls or roof planes. Blank, windowless walls are not permitted.

Where solid walls are required, the wall should be articulated by the provision of blank window openings trimmed



with frames, sills, lintels, or, if the building is occupied by a commercial use, by using recessed or projecting display window cases.

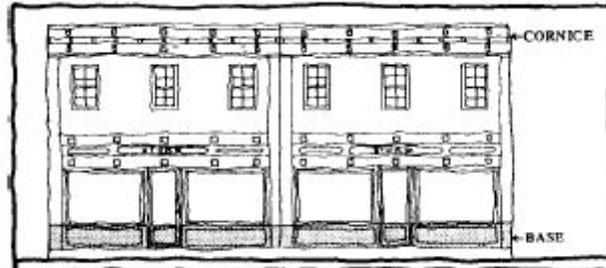
- The difference between ground floor commercial uses and entrances for upper level office or residential apartment uses must be reflected by differences in facade treatment. Storefronts and other ground floor entrances should be accentuated through cornice lines. Storefronts should be integrally designed as part of the entire facade.



- Façades shall be built parallel to the principal frontage line, if curved or broken the façade shall be built on a tangent line to it. Corner lots shall have a principal frontage determined by the Planning Board. In the case of an infill the setbacks shall match one or the other of the existing adjacent buildings.



- Buildings greater than one story should clearly delineate the boundary between each floor of the structure through belt courses, cornice lines, or similar architectural detailing. Structures may vary in height to add variety, with taller buildings placed at corners or points of visual termination. However, a consistent “build up line” should be maintained.

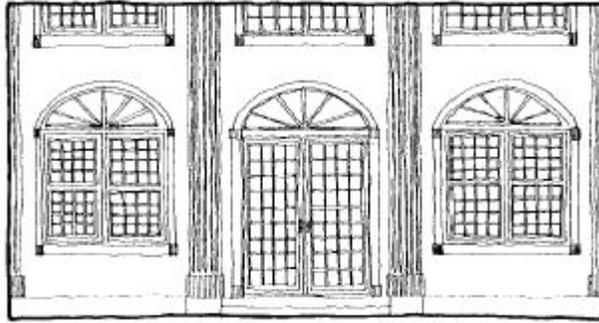


- A commercial or mixed-use building, such as a corner store, located in the Main Street District must integrate its appearance with the area and should not exceed twice the height and massing of adjacent buildings.

- Human scale should be created by building massing form, as well as the use of architectural elements such as colonnades, canopies, walkways, street-level display windows, lighting, and a variety of building

materials. Human scale should be further reinforced by site design features around the building exterior.

- Service and mechanical areas shall be designed as an architectural feature of the building and entirely screened from view.



- Pitched roofs, if provided, shall be symmetrically sloped. Parapets shall enclose flat roofs, a minimum of 42 inches high or as required to conceal HVAC equipment.

- The location and design of exposed leaders and downspouts shall be identified on plans submitted for Planning Board approval. Leaders and downspouts shall drain into drywells. When exposed leaders and downspouts are necessary on any building elevation, they shall be designed as an architectural element of the building.

- Water from downspouts must be routed under or around sidewalk areas and must not flow over walkways where it could freeze and create dangerous walking conditions.

- If downspouts cannot be designed to blend with the building's style or theme and avoid a "tacked-on" or mechanical appearance, then the downspouts shall be closed and entirely screened from view.

ii. Architecture:

- Architectural elevations shall be submitted for review and approval to the Department of Planning, Environment and Development at the time of site plan submission.

- Consistent architectural design, including building materials and colors, should be carried throughout the development area. Designs

should provide visual interest and variety, yet be consistent with the architectural character of area.

- Building materials should be similar to the materials of structures in the area. However, dissimilar materials may be permitted when incorporating other characteristics such as scale, form, architectural



detailing and color to make the building compatible with the area.

- Materials requiring low maintenance are recommended over high maintenance materials. For instance, materials with integral color are generally recommended over materials that require painting.

- Attached buildings within the same block must contain consistent cornice lines. Architectural embellishments that add visual interest to roofs, such as dormers, belveders, masonry chimneys, cupolas, clock towers, and other similar elements are encouraged.

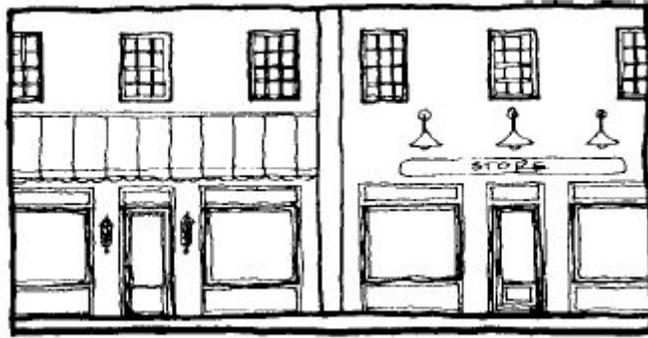
- Buildings with multiple storefronts should be unified through the use of architecturally compatible materials, colors, and details.



- Buildings must be designed to create street level interest and pedestrian comfort. Doorways, covered walkways, windows, and other street level ornamentation should be incorporated to create pedestrian scale and inviting spaces.

- Commercial structures should provide awnings, covered walkways, colonnades, or other weather protection. Ground floor retail,

service, restaurant, and other commercial uses must provide large pane display windows on a minimum of 50



percent of the front facade to provide views into the interior of buildings.

- Ground floor retail, service, restaurant, and other commercial uses must provide large pane display windows on a minimum of 50 percent of the front facade to provide views into the interior of buildings. Windows shall be clear glass.

- Awnings should be used to complement the architectural style, materials, colors, and details. Canvas or other similar waterproofed fabrics are preferred. Fixed or retractable awnings are permitted at ground level and on upper levels where appropriate.

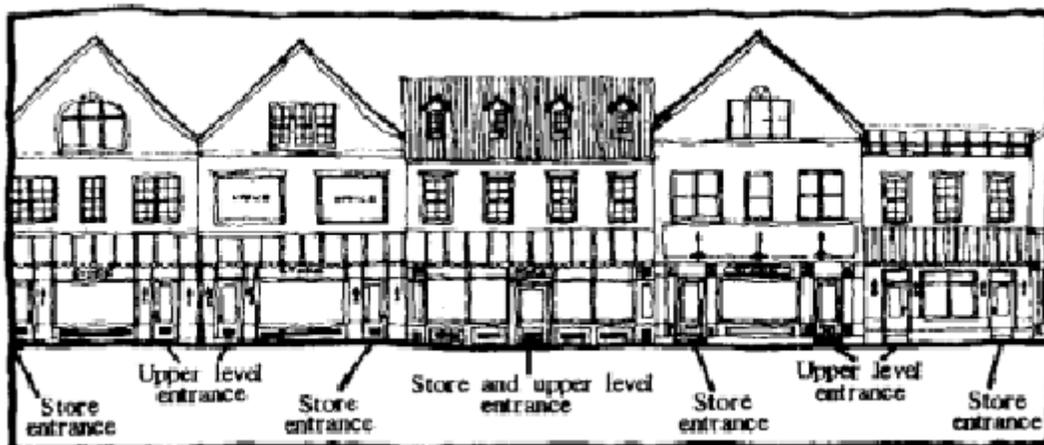


- Entrances and storefronts must face the street. All entrances to a building must be defined and articulated by architectural elements such as columns, porticoes, porches, overhangs, railings, balustrades, and other similar elements. Doors, windows, balconies, porches, and roof



decks should be oriented toward the street and other public spaces to encourage social interaction.

- In mixed-use buildings, the difference between ground floor commercial uses and entrances for upper level commercial or apartment uses must be reflected by differences in facade treatment. Storefronts and other ground floor entrances should be accentuated through cornice lines. Storefronts should be integrally designed as part of the entire facade. Buildings with multiple storefronts should be unified through the use of architecturally compatible materials, colors, and details. Canvas or other similar waterproofed fabrics are preferred. Fixed or retractable awnings are permitted at ground level and on upper levels where appropriate. Awnings should complement the architectural style, materials, colors, and details. Canvas or other similar waterproofed fabrics are preferred.

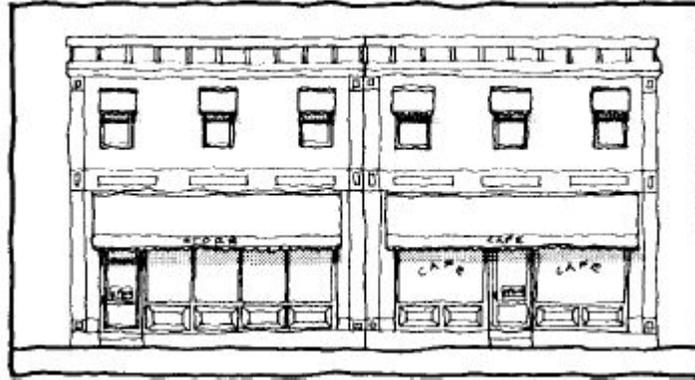


iii. Signage

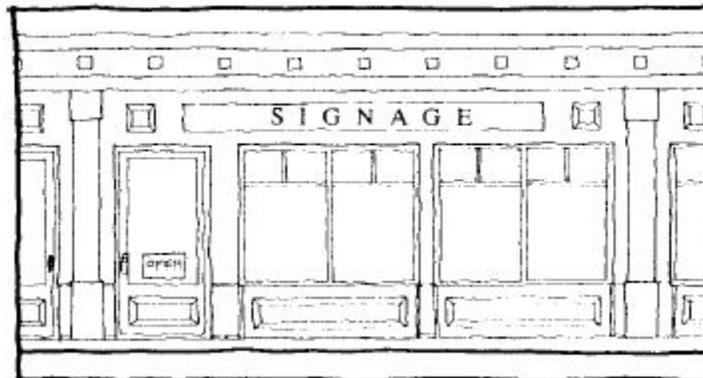
- Signs are limited to wall, awning, hanging signs, or other similar accessory signs of low scale and low visual impact. The signs must compliment the building's architectural style.

- One (1) wall sign attached to or incorporated into each exterior side wall of each store facing a public street or parking area, advertising only the business conducted in such store.

- One address number no more than 6 inches high shall be attached to the building in proximity to the principal entrance.



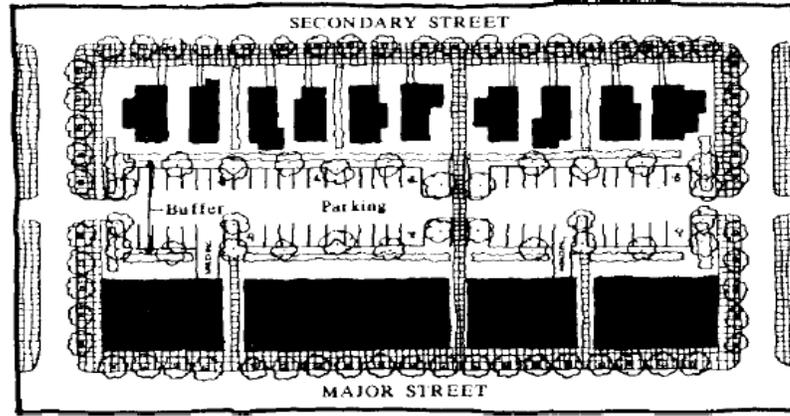
- One blade sign for each business may be permanently installed perpendicular to the façade. Such sign shall not exceed a total of 4 square feet unless otherwise specified by the Town Board.



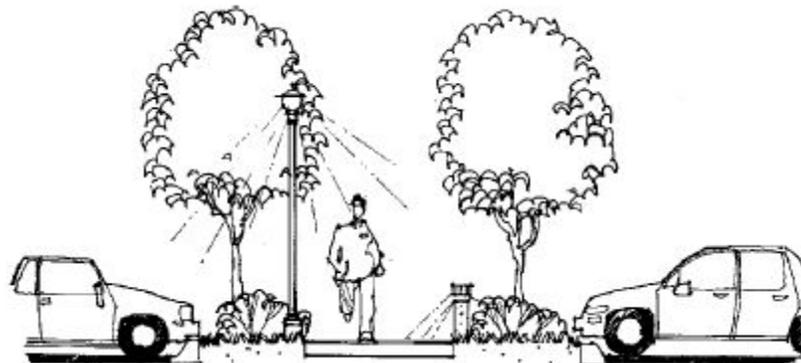
- A detached or ground sign could be permitted by the Planning Board, advertising only the assembled businesses conducted on the premises upon which the sign is located. Individual sites should not be permitted ground signs.
 - Signs shall be lit with external building mounted lighting fixtures and shall not be backlit.
 - All signs should be of wood or the Planning Board may approve similar materials as deemed appropriate.

iv. Parking:

- Access and parking shall be located with and/or shared with adjoining properties.



- Parking lots must be located behind buildings. When located along the side of a structure, the parking lot must be screened with landscaping or with streetwalls.
- Rear vehicular access is required within the Main Street District. Curb cuts on the Main Street should be discouraged or prohibited.
- Parking lots shall be designed to separate pedestrian traffic from vehicle traffic, and include pedestrian walkways within the parking areas to direct pedestrian traffic to store entrances.
- Where walkways pass through paved parking areas and drives, the following shall be accomplished: A decorative paving material (i.e. pavers, stamped/textured concrete, or color concrete) shall be used to delineate the walkway crossing. The corners of the intersection should be



flared (narrow driveway/street width at crossing). ADA ramps shall be used at all crossings. Walkways shall be designed and buffered in a manner that encourages their use.

- Loading docks and service areas shall not be permitted on the frontages.

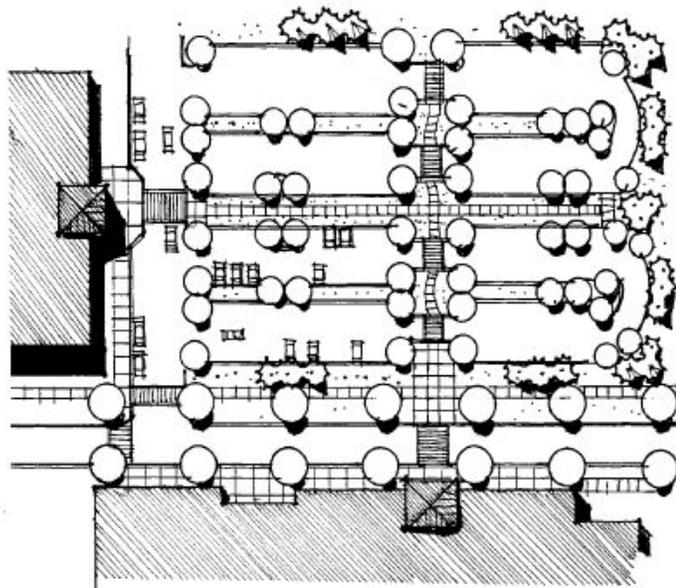
- On-street parking along the frontage lines corresponding to each lot shall be counted toward its parking requirements.

- A public parking lot or possibly a public parking garage will be provided.

- Parking internal to and to the rear of the buildings, fifteen foot alleyways to the street fronts.

- Access to transit stops and neighborhood retail centers, whenever possible.

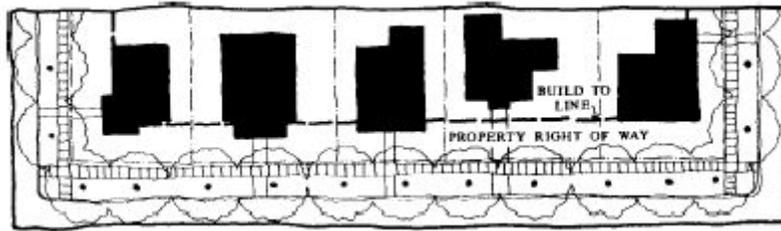
- Aisles should be placed on both sides of entrance drives to create pleasing tree lined entrances, to direct vehicles into and out of the site, and to provide adequate space for vehicular stacking at exits onto perimeter roadways.



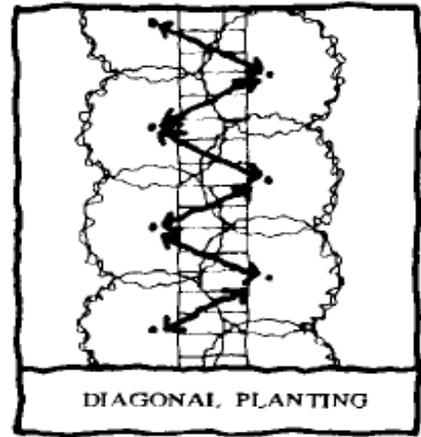
v. Landscaping and Buffering:

- All yard areas shall be planted in conjunction with an approved detailed landscaping plan and shall be maintained in a neat and attractive manner.

- A minimum of 1 street tree is required per lot, with a minimum of 2 trees required on a corner lot. A preferred tree species list is provided herein.



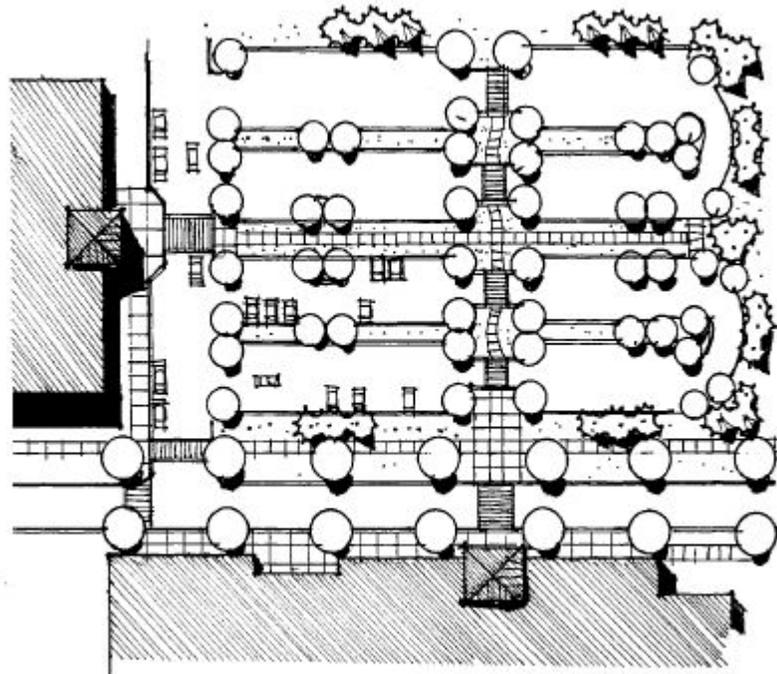
- Street trees shall be required and located within the right-of-way and between sidewalks and curbs within a 4 foot wide landscape cutouts. Street trees shall be a minimum of a four (4) inch caliper, every thirty (30) feet on center along the sites frontage. Planning Board variances in street tree spacing may be necessary to coordinate utilities, streetlights, driveways, storm drain structures, sidewalks, and traffic clearance zones. It may be necessary to group trees in some locations to avoid obstructions.



- Street trees shall be located within the right-of-way and between sidewalks and curbs, or along the outside of the right-of-way within a 4 foot wide landscape cutouts.

- The developer shall be responsible for planting street trees and landscaping at the prior to the issuance of a certificate of occupancy.

- Street trees should be spaced as uniformly as possible. However, it may be necessary to group trees in some locations to create views and to avoid obstructions.
- Variances in street tree spacing may be necessary to coordinate utilities, streetlights, driveways, storm drain structures, sidewalks, and traffic clearance zones.
- Street tree species list is provided herein.
- Landscape design and species shall be used to create visual continuity throughout the development.
- Landscape areas should be combined to form larger clusters at highly visible locations such as landscaped courts, plazas or common areas.
- Landscape design should create variety, interest, and view corridors for visibility.



- A variety of different species (including both deciduous and evergreen species) shall be incorporated into the site design to provide visual interest, as well as disease and pest resistance.

- The five-foot front yard setback area shall be a planted landscaped area or surfaced to provide for outdoor seating or retail display area. Foundation planters and trees should be incorporated around the building exterior to soften the building appearance and to create a place of relief from the summer sun.

- All parking areas shall be screened from view with a hedge, berm and/or decorative wall or fence in accordance with Town standards.

- Parking areas of fifty (50) spaces or more shall contain four hundred (400) square feet of landscaping for each twenty-five (25) spaces. Large parking areas shall be divided into smaller parking fields of 50 cars with landscape strips, peninsulas, or grade separations to reduce the visual impact of large expanses of paving, to direct vehicular traffic through the parking lot, and to provide a location for pedestrian walks. This shall not apply to structured parking (e.g., parking garages).

- A buffer area of twenty five (25) feet shall be maintained adjacent to any residence district. Natural buffer areas, which do not maintain a density and quality of plantings equal to a single row of evergreen plantings five (5) to seven (7) foot in height and five (5) feet on center, shall be supplemented with additional plantings in order to meet minimum buffer requirements.

- All landscaping and buffer areas shall be irrigated in accordance with Town standards.

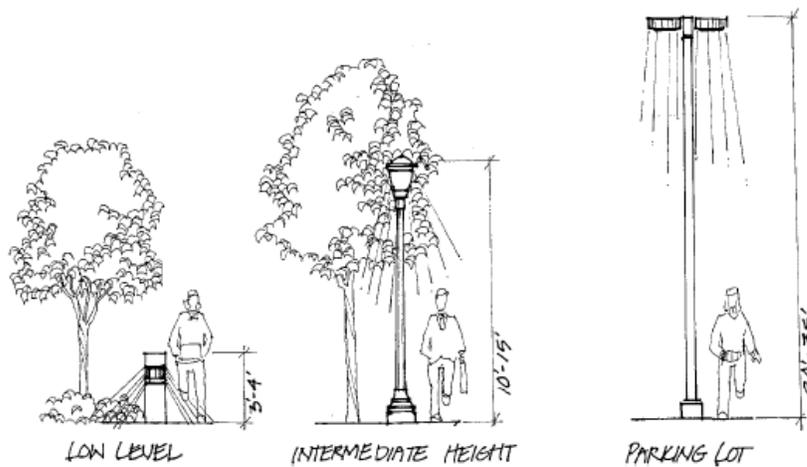
vi. Lighting:

- Decorative streetlights of not more than twelve (12) feet in height, shall be uniformly applied, installed and maintained situated in sidewalk cutouts every thirty (30) feet on center along the sites frontage. The same lighting fixture shall be applied to each main street district. Street light fixture styles shall be one of the following: Battery Park Fixture, Pennsylvania Globe Nantucket model, Other models as may be approved

by the Planning Board as acceptable to the Town Of Brookhaven Division of Street lighting.

- Building-mounted light fixtures should be for aesthetic and safety purposes only and must direct light upward or downward. Lighting should be used to highlight architectural features and create visual interest. Wall-pack lights or other lighting that shine outward toward adjoining properties or street right-of-way is prohibited.

- Accent lighting that highlights building architectural features is encouraged. Exterior neon lighting, illuminated banding, or other lighting that creates a glow is prohibited, unless approved by the Planning Board as part of an overall theme for the development area.



- Parking lot illumination must be accomplished with individual light poles and fixtures. Building-mounted fixtures are not permitted as a method of parking lot illumination.

- The style of lighting should reflect the architectural character of the area.

- Maintain parking lot light poles/fixtures of the same style, height, color, and intensity of lighting throughout the development area. Varying styles of fixtures may be permitted if it is demonstrated that the styles contribute to an overall theme for the area.

- Decorative off street parking lot lights of not more than eighteen (18) feet in height, and 400 watts or less shall be uniformly applied, installed and maintained.

- Off Street Parking Lot light fixtures shall be non-adjustable horizontally mounted fixtures with less than 90-degree luminaire cutoff. Fixtures that project light or glare toward street right-of-way or adjoining properties shall not be permitted.

- Pedestrian lighting shall be uniformly applied, installed and maintained along pedestrian walkways and alleyways.

- Building-mounted light fixtures should be for safety or signage purposes only and must direct light downward. Wall-pack lights or other lighting that shine outward toward adjoining properties or street right-of-way is prohibited. Shielding shall be provided to avoid light trespass and glare.



- Exterior neon lighting, illuminated banding, or other lighting that creates a glow is prohibited, unless approved by the Planning Board as part of an overall theme for the development area.

vii. :Street Furniture.

- The Planning Board may permitted and/or require one or more of the following street furniture amenities: Street Planters, Hanging Flowers/Plants, Benches, Waste Receptacles, Community Signs/Bulletin Boards,



Information Kiosks, Bollards & Bicycle racks.

viii. Pedestrian.

- Decorative paving materials and pedestrian amenities, pedestrian light poles, bollards, seating, bicycle racks and trash receptacles should be used to accent pedestrian spaces at building entrances, courts, plazas and along pedestrian walks.

- Site design should separate pedestrians and vehicles as much as possible, with the number and length of pedestrian crossings through parking and paved areas kept to a minimum. Where walkways pass through paved parking areas and drives, the following should be accomplished.

- A decorative paving material (i.e. pavers, stamped/textured concrete, or color concrete) should be used to delineate the walkway crossing.



- The corners of the intersection should be flared (narrow driveway/street width at crossing).

- ADA ramps shall be used at all crossings. Walkways shall be designed and buffered in a manner that encourages their use.

- A Ten foot sidewalk plus a four feet planter area for street trees and street furniture shall be required.

ix. Multi Family Residential:

- An internal vehicular circulation system reflective of a single-family residential street system, as opposed to looped systems which may appear disjointed and confusing.
- Buildings sited with front entrances and porches oriented toward streets, drives, and plazas, rather than clustered around parking lots.
- Walkways that connect all buildings with parking areas, play areas, clubhouses, and sidewalks along adjoining streets, as well as neighboring stores, offices, and transit stops.
- Centrally located plazas, clubhouses, pools, and recreational facilities.
- Side and rear building elevations, garages, carports, and all accessory structures with the same level of design, aesthetic quality, and architectural detailing.
- Porches, varied rooflines, and varied façade depths to create variety and individuality of each dwelling within the building.
- Garages designed to be integrated with the building design or sited so as to avoid long monotonous rows of garage doors and building walls. Garages shall be oriented so that they do not visually dominate the building façade or the streetscape.

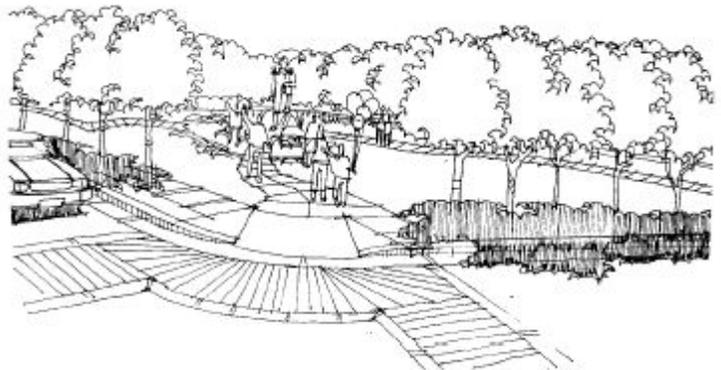
D. Parks and Open Space.

Public and private park and civic uses are an important component to any

Main Street District.

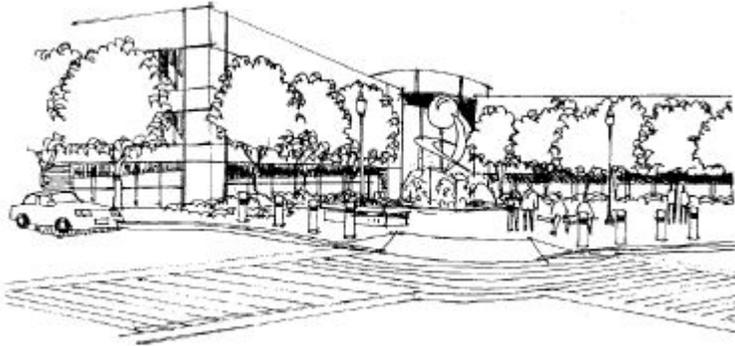
Several vacant parcels have been identified as potential park, greenway or civic land uses. Well-configured

public spaces such as squares, plazas, greens,



landscaped streets, greenways, and parks are woven into the pattern of the Main Street District and dedicated to the social activity, recreation, and visual enjoyment of the residents of the Town of Brookhaven. The neighborhood design creates a hierarchy of useful open spaces: a formal square in the Main Street District; parks and playgrounds throughout the Main Street District; and street environments designed to promote walking and casual meetings between residents.

Landscape and open space is one of the most important features in a Main Street District. Open space, both public and private, is distributed

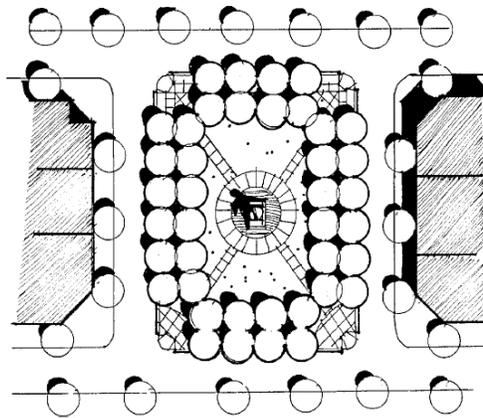


throughout the district and provides not only aesthetic qualities, but recreational and civic functions as well. A variety of open space should always be conveniently accessible to all residents and provide physical linkages throughout the neighborhood. It should also provide gathering places for residents and places for social interaction.

Limited opportunities for large areas of open space exist within the Study Area, however a larger public park space and greenway is recommended along the Forge River. This will require the acquisition of two key parcels of land. One of these parcels, located along Montauk Highway, is presently used as a fence company with parking area. This site is ideal for providing the public parking and access to the natural areas. Neighborhood Park and playground and other similar improvements can also be included on this parcel. The other larger parcel is a very sensitive parcel of land. The 33-acre parcel has been the subject of a myriad of development plans and has remained on public acquisition lists for decades. Wetlands have been identified on this site and have consistently been on the recommended acquisition list. The development of a passive and active

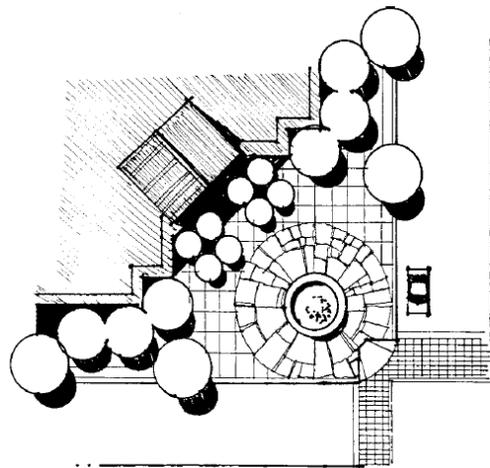
recreational park and nature center in this area will serve to provide recreational opportunities for residents and preserve significant scenic vistas.

Open space ranges from public and semi-public areas such as neighborhood parks, squares, plazas, greenways, parkways, and other community common areas to private areas such as rear yards and courtyards. In the residential areas with small lot sizes or higher density buildings, public open spaces are critical components. Open space can be dedicated to the Town of Brookhaven or maintained under private ownership.



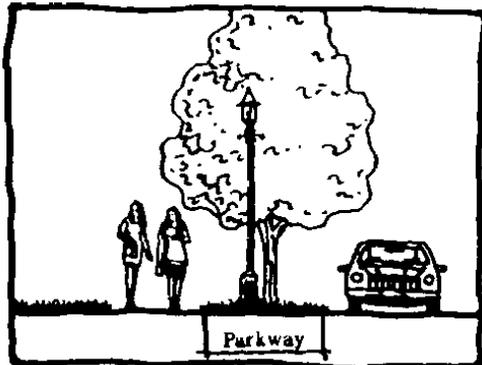
The neighborhood park is the focal point and foundation of the community. It provides an area for recreation and play, as well as social activities such as community picnics, bazaars, holiday displays and activities for all ages to meet and gather together. The park is best located where it is accessible to the highest possible number of residents.

A square is a type of open space or neighborhood park that may encompass an entire block. It is located at the intersection of important streets or in the Main Street District and is intended for civic or public purposes. The square is typically developed in a formal manner consisting of paved walks, lawns, trees, fountains, and may include civic buildings. Civic buildings may also be located across the street from the square. This area is intended as a central gathering space for the community and should be designed to accommodate a wide variety of gatherings.



A plaza is a form of open space that is typically more urban in nature and occupies a smaller portion of a block than a square. Plazas are typically located at the intersection of major streets. These spaces may range from highly interactive areas with adjacent commercial uses, such as retail shops and sidewalk cafes, to quiet passive areas for sitting, reading and relaxation. Plazas are designed in a formal manner with landscaping, seating and other amenities such as fountains and public art.

An open space area or greenways are natural areas where recreation and conversation of nature are among the primary values. They are fingers of green in varying shapes that follow natural features such as ravines, creeks, streams, and other watercourses.



The parkway is public open space or a planting strip, located between the sidewalk and the street. It is important in defining the pedestrian realm by separating it from the street and creating a feeling of safety and comfort. The width of a parkway varies from four to fifteen feet depending on the type of

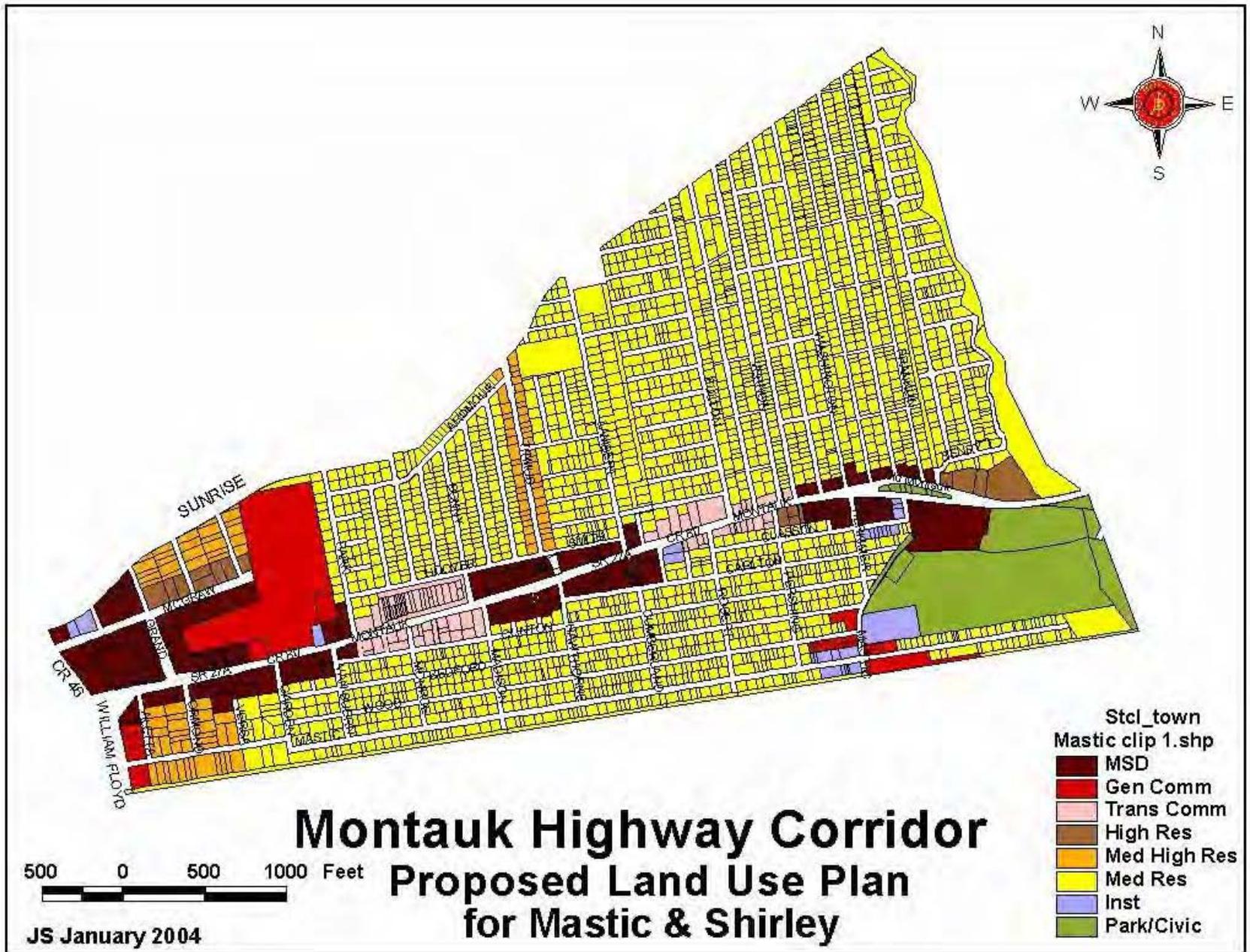
street and adjoining land uses. Grass, shrubs, flowers, street trees, and street lighting are located within the parkway. Utilities will very rarely be located within this area.

Street trees are required in the Main Street District to create a frame around and improve the appearance of the Montauk Highway Corridor, as well as, separate vehicles from pedestrians. The existence of trees on the street side of the pedestrian sidewalk is critical. Whenever possible, the placement of trees on both sides of a sidewalk creates an exceptional pedestrian environment. In a mixed residential area street trees shall be located in the parkway within the street right-of-way and spaced approximately 20 feet on center. However, it may be necessary to group trees in some locations to avoid obstructions and create views to retail shops.

A concerted effort is required between community leaders, local government and landowners to site civic or park uses in or adjacent to the Main Street Districts. Presently the Colonial Youth Organization plans to build on the north side of the Sunrise Highway North Service Road. The CYO provides day care and athletics to approximately 1,000 youths. An organization such as CYO should be encouraged to site within or adjacent to the downtowns in order to encourage pedestrian activity and provide economic support to local retail and service providers. The existing Mastic-Shirley Library is located on a small out-parcel, within a commercial center, prohibitively far from the main street districts. Library expansion at its present location is constrained by parking requirements, as the Library parcel has no parking field, nor area to provide parking. It appears prudent that plans should be developed to accommodate the future expansion of the library or library annex within walking distance of one of the main street districts.



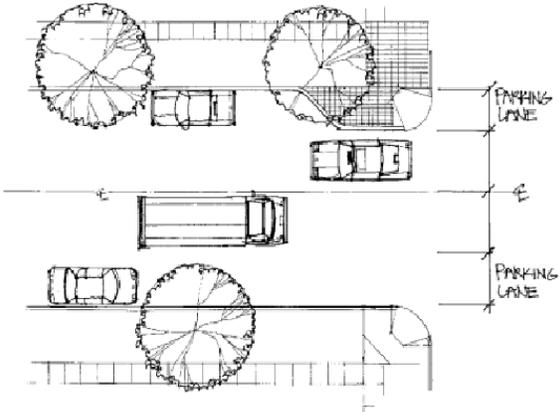
Figure 09



2. Infrastructure

A. Transportation.

The transportation improvements, specifically to the Montauk Highway Corridor, will necessitate a joint and concerted effort between the Town Of Brookhaven, Suffolk County Department of Public Works and New York State



Department of Transportation. The improvements proposed should consist of a pavement cross section including travel lanes and streetscapes that presently do not exist. The travel lanes should provide the capacity for vehicle and bicycle traffic and include some on-street parking areas. The streetscape will contribute to the character of the community. The pavement cross section includes a variety of lane widths to accommodate moving vehicles (12 ft) and bicycles (5 ft) with provisions for turning lanes (12 ft) where needed and on street parallel parking (8 ft), in certain areas within the Main Street District. Streetscapes should include curbing, street trees, sidewalks, planters and street furniture. The corridor should incorporate a raised landscaped center median separating opposing travel lanes with emergency vehicle cutouts. The raised median should also be designed to allow for emergency vehicles to cross by using mountable curbing.

Implementation of these recommendations will provide for the means to handle the vehicle volumes and demands of vehicle mobility while providing the design elements and necessary enhancements to the corridor. Alternate routes of access, parallel to and perpendicular to the corridor, will be accomplished by developing the existing surrounding street grid layout. There are several short sections of mapped roads the connect Clinton, Classon

or Carlton Avenues to Montauk Highway, that detract from safe vehicle circulation and compromise the intent of the Federally funded improvements to County Road 80. The discontinuance and abandonment of these portions of the roads accessing Montauk Highway should be accomplished to facilitate access management and eliminate points of conflicts. These roads have been identified herein. Alternate access routes will mitigate the potential for adverse corridor delays. A level of acceptable delays with minimum reduction of mobility will be satisfactory in order to provide a transportation corridor that enhances overall traffic safety by reducing traffic speed, reducing accident potential and severity and increasing the corridor's friendliness for bicycle and pedestrian activity. The proposed transportation network will improve overall mobility throughout the corridor.

Traffic safety measures should include the use of traffic calming techniques including the use of roundabouts and coordinated traffic signal systems, as well as providing enhanced pedestrian crossings utilizing curb extensions and crosswalk signs, markings and variable pavement surfaces at key intersections. Bike lanes and/or routes are proposed throughout the Montauk Highway Corridor. Public transportation will continue to be available and user enhancements will be provided at specific locations where bus stops, bus turnouts and bus shelters should be provided.

The improvements proposed along the Montauk Highway corridor will vary throughout the length of the corridor. The corridor design should provide the availability of on street parallel parking located in specific areas within the main street districts. A different corridor design will be established in the "transitional" areas outside of the main street districts. In these areas the center medium should continue but with no on street parking. As opposed to the designated main street districts where curb cuts will be minimized or even eliminated completely. In the transition areas individual curb cut access to Montauk Highway will be necessary. Bike lanes and street trees should be

provided throughout the corridor. Outside of the Montauk Highway Corridor an enhanced street grid network should be provided.

It is extremely important to the functionality of a main street district to minimize the amount of street pavement and separation or distance between the buildings fronting along the right-of-way. This will help ensure that pedestrian activity will occur along both sides of the roadway and reduce the amount of vehicular trips from one part of the main street district to another. This design element also minimizes the distance to cross the street and helps to provide for a more efficient traffic controls along the corridor. The recommendations contained herein provide the means to accomplish the goals of this plan while providing for the necessary improvements to the Montauk Highway Corridor and other supporting roadways.

i. Improvement Recommendations

This section intends to provide the recommendations necessary for the improvements of certain roadways within this area, particularly the Montauk Highway Corridor and the alternative roadway network. Typical roadway cross sections are provided as well as specifications for traffic calming, streetscape and pedestrian enhancements.

Four roundabouts are proposed to enhance traffic movements and mobility. The first is located at the McGraw Street and Grand Avenue intersection, the second at Montauk Highway and Titmus Drive/Hawthorne Street intersection, the third at Montauk Highway and Fulton Avenue intersection and the fourth at the intersection of Montauk Highway and Herkimer Street/Washington Avenue.

An integrated roadway network is proposed utilizing existing streets to the north and south, east and west of the corridor, which would serve as alternate routes to Montauk Highway and William Floyd Parkway to travel east and west, north and south within as well as to and from the corridor. These include McGraw Street easterly from William Floyd Parkway to the northerly entrance to the Southport shopping center. Connecting the proposed rear yard

parking areas and eliminating individual curbs cuts on Montauk Highway within the main street districts will also provide alternative travel routes for vehicles.

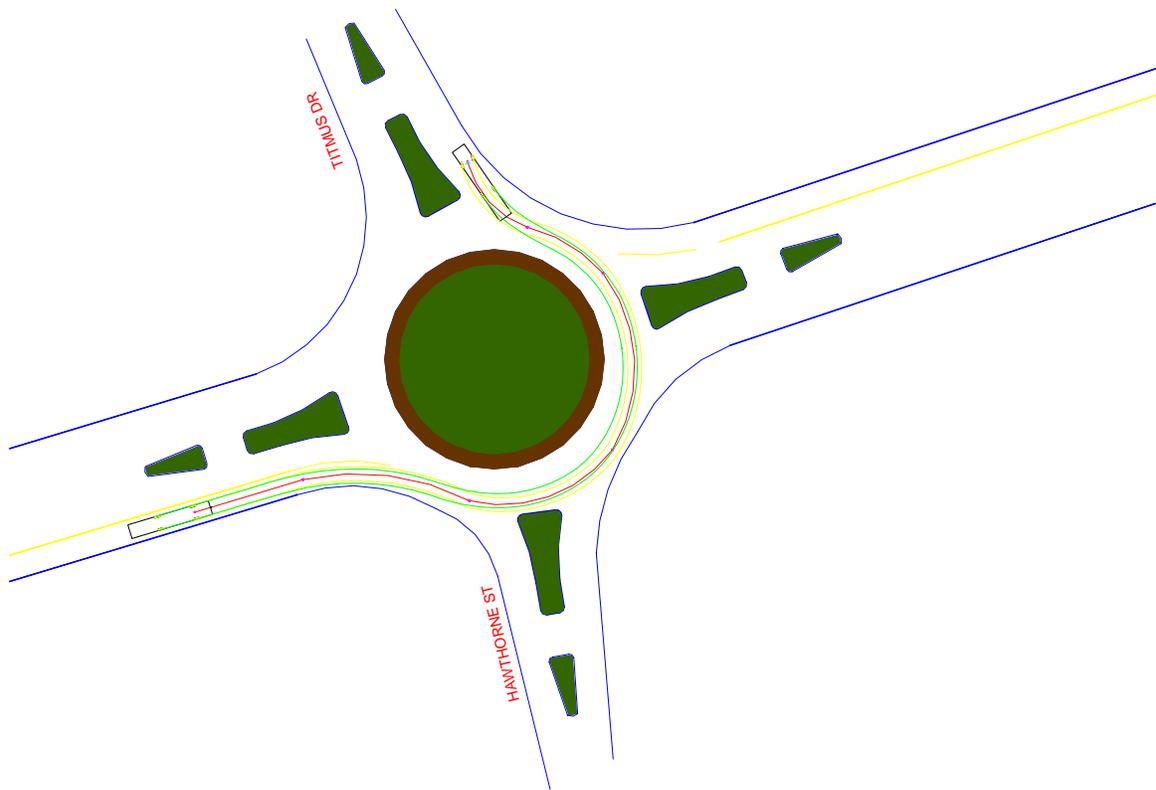
The integrated roadway network includes the improvement of Hoover Street/Smith Street between Park Avenue and Lambert Avenue east and west of Titmus Drive, 1st Place between Lambert Avenue and Fulton Avenue, and Sunrise Highway South Service road between Park Avenue and Titmus Drive. All three of these parallel streets are located north of Montauk Highway. On the south side of Montauk Highway, Bedford Avenue from Garden Place to Hawthorne Street, Clinton Avenue from Van Buren Place to Hawthorne Street and finally, Mastic Boulevard from Hawthorne Street to Mastic Road offer additional east-west connections.

Additional roadway network improvements could include the use of Aletta Place/Mastic Blvd. as an alternative access from Montauk Highway to and from William Floyd Parkway. Camp Upton Road to the west of William Floyd Parkway could also be used as an alternative access to William Floyd Parkway and a connector to McGraw Street.

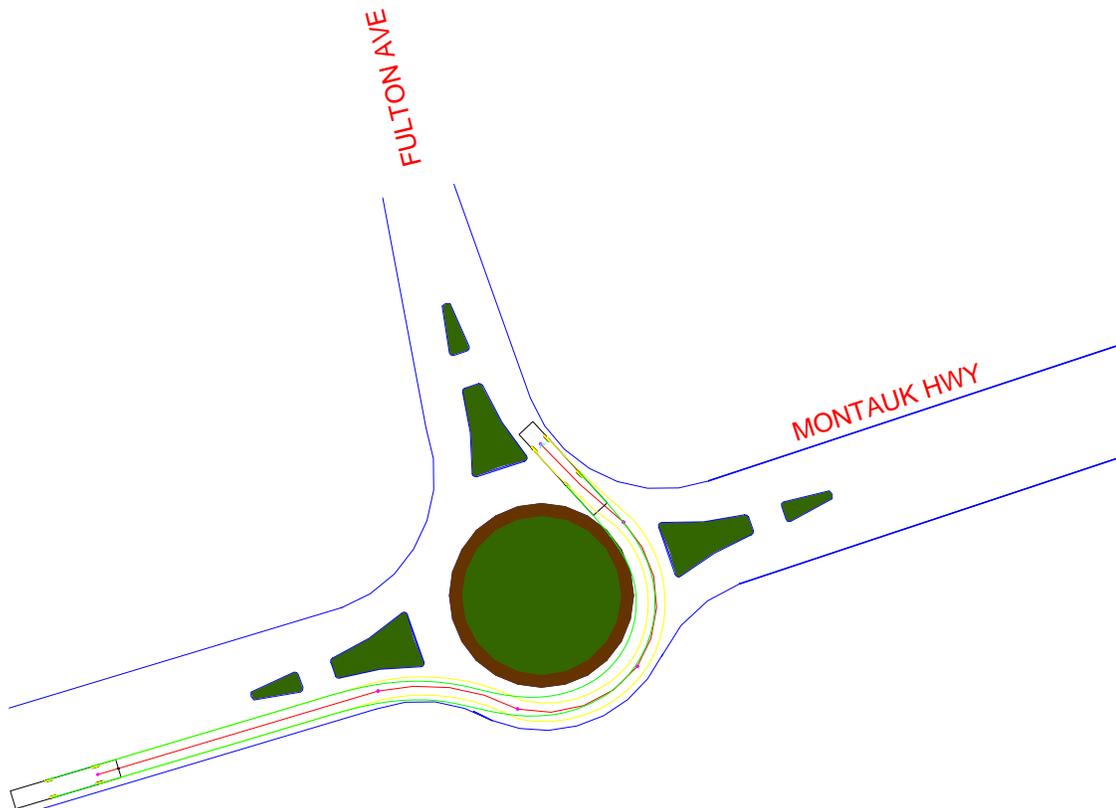
The existing street grid layout also offers alternatives for north/south traffic movements. These include the improvements to Grand Avenue from Montauk Highway north to McGraw Street, Garden Place south from Montauk Highway to Bedford Avenue. Improvements to Park Avenue from Montauk Highway north to Sunrise Highway South Service Road and Titmus Drive from Montauk Highway north to Sunrise Highway are all vital in providing the needed street connections. The Town of Brookhaven recognizes the need for a new access to the Sunrise Highway main line and/or future service roads of the Sunrise Highway main line at Titmus Drive. Furthermore we support design alternatives for an interchange at this location.

Another vital street connection requires the improvements of Hawthorne Street from Montauk Highway south beyond the Montauk Branch of the Long Island Railroad, which will require the at-grade crossing of the existing railroad tracks. This endeavor may be challenging at the least, since

additional at-grade crossings are typically unfavorable to the Long Island Railroad (MTA). However, presently there are only two railroad crossings in this area, one at William Floyd Parkway to the west and the other at Mastic Road to the east of Hawthorne Street. Considering that the vast majority of the population of the Mastic-Shirley area resides south of the Long Island Railroad, it is vital to consider an additional crossing.

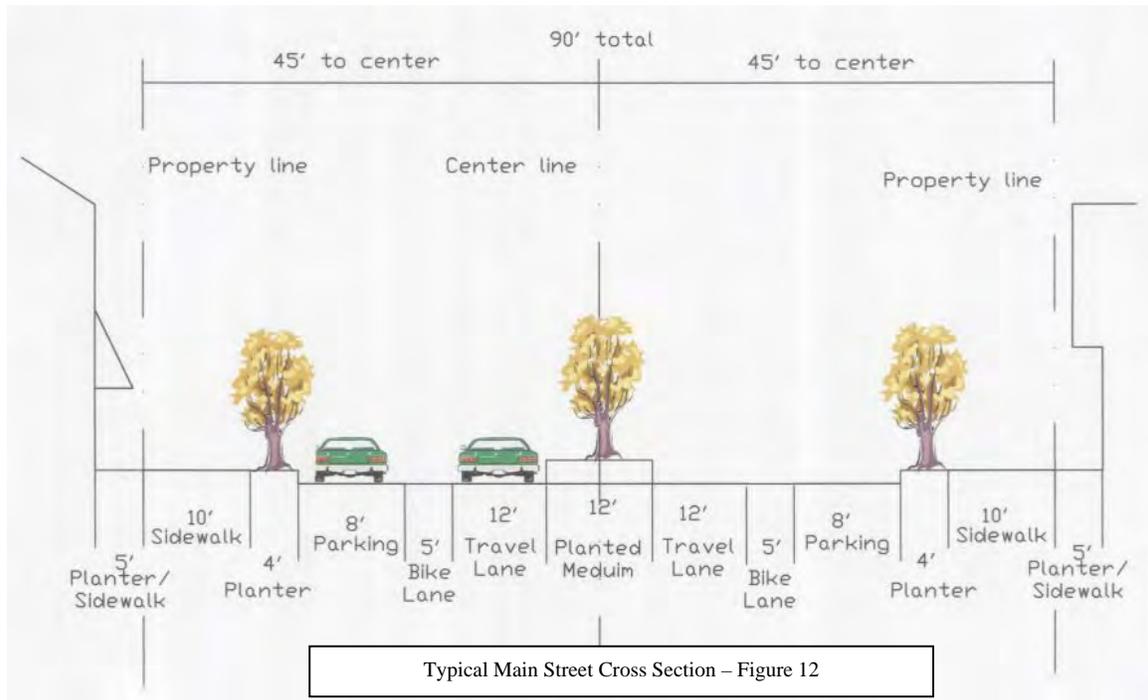


Figures 10 & 11



(a) Montauk Highway Corridor

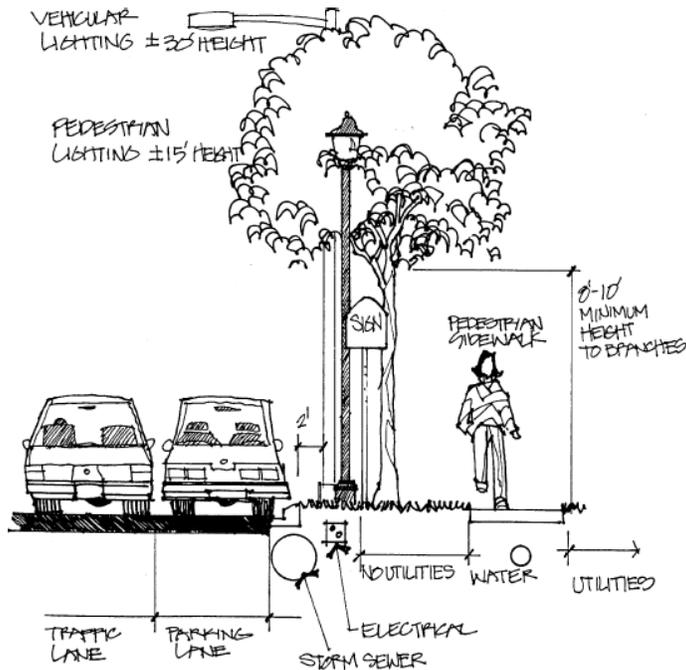
The Montauk Highway Corridor within the Main Street District should be redeveloped and improved as a boulevard with raised landscaped median, vehicle travel lanes, bicycle lanes on both sides of the roadway, curb and sidewalks, street trees and decorative streetlights. On-street parallel parking should be made available on both sides of the corridor within the main street districts. In selected areas along the corridor the raised landscaped median should provided emergency vehicle cutouts. These cutouts should be angled and designed to allow emergency vehicles only, they should also be deigned in an attractive manner with brick pavers or similar materials. Also in selected areas, the on-street parking areas within the main street district will be discontinued to provide for bus stops with bus lane turnouts where bus shelters will be provided. A typical cross section shows how the various roadways can be developed, see Figure 12.



As previously stated, roundabouts should be developed at the Titmus Drive/Hawthorne Street intersections, Fulton Avenue intersection and the

Herkimer Street/Washington Avenue intersection, see figure 10 & 11. At other critical intersections, where additional traffic controls are required, traffic signals should be provided. These include the Grand Avenue and Garden Place intersections along Montauk Highway, which are currently signalized. Other intersections that may require traffic signal control as may be determined by Suffolk County DPW. Also at critical intersections, pedestrian crosswalks should be provided with all required pedestrian and traffic controls.

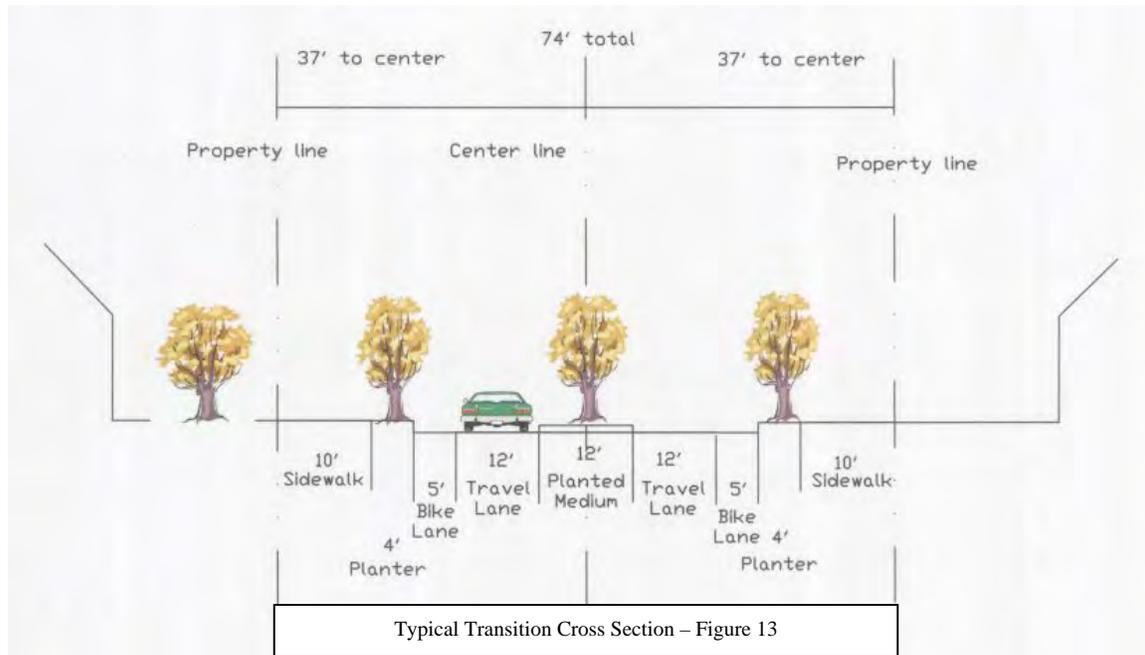
Public utilities are currently located above ground within the entire length of the Montauk Highway corridor. The goal of this project must consider the relocation of all utilities below ground. This will eliminate many roadside hazards and significantly improve the visual quality of the corridor.



Access management throughout the corridor should be implemented, which may eliminate, limit and/or consolidate the multitude of individual site curb cuts. Common driveways and shared access agreements need to be considered. Access to the parking areas located at the rear of the buildings should be provided through the side street network. Several unimproved

streets and some substandard but improved roadways should be eliminated or closed. These include Versa Place, Park Place, Haven Place, Cedar Place and Leerets Place. Along these former street right-of-ways,

alleyways should be provided for pedestrian access from the rear parking areas to the street frontage.



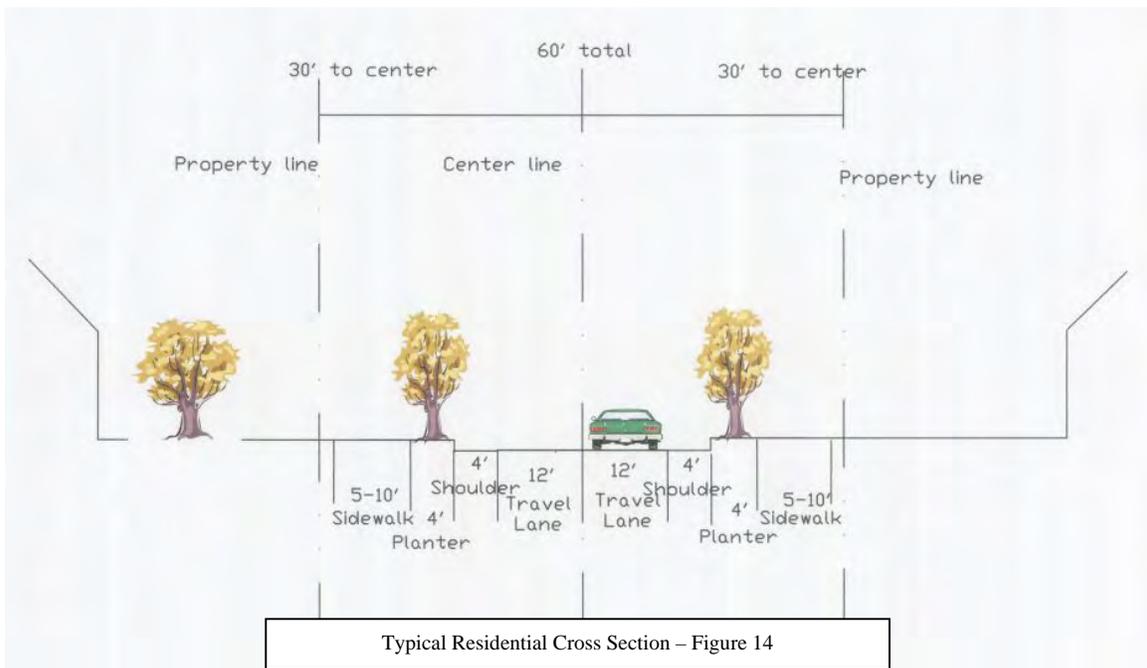
Within the transitional areas, outside the Main Street Districts, the Montauk Highway corridor should be designed as a two lane (one lane in each direction) roadway with bike lanes in each direction with curb, sidewalks, street trees and decorative streetlights. A typical cross section is provided that shows how this segment of the roadway can be developed (see Figure 13.) Transitional areas will be developed predominately for single-family residential uses or those uses consistent with the “J” Business District with many of the uses having individual driveway curb cuts as may be required. The pavement cross section includes a variety of lane widths to accommodate moving vehicles (12 ft) and bicycles (5 ft) with possible shoulders. Streetscapes should include curbing, street trees, sidewalks, planters and street furniture. The corridor should incorporate a raised landscaped center median separating opposing travel lanes with emergency vehicle cutouts.

(b) Other Main Street Corridors

The other Main Street Corridors will be improved with typical enhancements to enrich the streetscape and the quality of life, as well as to provide the means for pedestrian activity and mobility for all users within the area. Most of these proposed improvements would be achieved within existing rights-of-way.

- McGraw Street will have a typical main street cross section with one travel way in each direction, raised center medium, on-street parallel parking. Curbs, sidewalks and decorative streetlights will also be provided. The provision of a bike lane in each direction may be considered. The present eighty (80) foot right-of-way may require an additional taking of five (5) feet on either side for these improvements.

- Grand Avenue will have a typical main street cross section with one travel way in each direction, raised center medium, on-street parallel parking. Curbs, sidewalks and decorative streetlights will also be provided. The provision of a bike lane in each direction may be considered. The present one hundred (100) foot right-of-way is more than enough to provide these improvements.



- Titmus Drive is presently a variable width right-of-way, however the majority of the roadway is a fifty (50) feet wide with a typical thirty four (34) feet of pavement with no curbs and sidewalks. The improvements proposed to Titmus Drive will require the taking of approximately five (5) feet on either side of the road in order to provide for a sixty (60) foot right-of-way. The additional right-of-way will accommodate the improvements planned, which include curbs, sidewalks, street trees, decorative streetlights and on-street parallel parking for the use of the residents that live along this roadway. Additional ten (10) feet of right-of-way may be needed if a bike lane is considered along this roadway. These improvements will improve the streetscape of the roadway, improve mobility for all users and enrich the quality of life for those residents living along this heavily traveled roadway.

- Hawthorne Street is presently improved from Montauk Highway southerly and terminates at the Montauk Branch of the Long Island Railroad. South of the railroad tracks, Hawthorne Street is improved southerly to Somerset Avenue. Hawthorne Street should be improved (within its present right-of-way of fifty feet) with curbs, sidewalks and decorative streetlights from Montauk Highway south to Somerset Avenue. A railway crossing should be considered to provide an additional railway crossing in this area. This crossing would have access and evacuation benefits for the entire Mastic-Shirley neighborhood to the south in the event of a natural disaster such as a 100-year flood. It should also be noted that Somerset Avenue is a major east-west connector roadway running between William Floyd Parkway and Mastic Road. The additional railroad crossing at Hawthorne Street will accomplish a much needed roadway connection and establish a valuable alternative street grid network for the Montauk Highway Corridor and Main Street Districts.

- Herkimer Street will have a typical main street cross section with one travel way in each direction, on-street parallel parking from Montauk

Highway to Classon Avenue. A raised center median may not be necessary along this section of roadway due to the short length of the proposed main street district, and the proposed round a bout. Curbs, sidewalks and decorative streetlights will also be provided. The provision of a bike lane in each direction may be considered. The fifty (50) foot right of way will require additional takings in order to accomplish these improvements.

- Mastic Road from Herkimer Street north to Montauk Highway could be completely abandoned and closed to Montauk Highway. All northbound and southbound traffic would than move through Herkimer Street. The existing residential dwellings on the side streets would access through Herkimer Street with cul-de-sacs established at their former Mastic Road intersections.

(c) Alternative Access Roads

Alternative access roads should be established and improved with enhancements and features that enrich the streetscape and the quality of life. These alternative access roadways will supplement the existing street grid and Montauk Highway Corridor to distribute and circulate all modes of transport. Vehicles, transit, bicycles, and pedestrian mobility will be enhanced along the Montauk Highway Corridor as well as throughout the neighborhood. Improvements along alternate access roadways would be achieved within existing right-of-way widths. Alternate access roadways include the following:

- Park Avenue should be improved with curb and sidewalks.
- Garden Place should be improved with curb and sidewalks.
- The Sunrise Highway South Service Road should be built and improved from Park Avenue easterly to Titmus Drive. This improvement should include curbs and sidewalks along the south side.
- Clinton Avenue should be constructed and improved from Van Buren Place easterly to Hawthorne Street. Presently this road is partially

improved east of Hawthorne Street and continues to Mastic Road. This roadway will also act as access to the parking areas established at the rear of the uses along the south side of Montauk Highway. Curb and sidewalks as well as street trees should be included in these improvements.

- Bedford Avenue should be constructed and improved from Garden Place easterly to Hawthorne Street. A small portion of this road is unimproved between Garden Place and Van Buren Place and should be improved. Curb and sidewalk improvements should be considered along this roadway.

- Carlton Avenue should be improved with curb and sidewalks from Hawthorne Street easterly to Herkimer Street.

- Smith Street should be constructed and improved from Titmus Drive to Lambert Avenue. These improvements should include curbs and sidewalks.

- 1st Place should be constructed and improved from Titmus Drive to Fulton Avenue. These improvements should include curbs and sidewalks.

- Hoover Court should be improved with curb and sidewalks as well as street trees.

- Aletta Place from Montauk Highway to Mastic Blvd. could be improved and used as an alternative access to William Floyd Parkway for all north and south turning movements. If this alternative is utilized the Montauk Highway Aletta Place intersection should be signalized and the proposed development along the north side of Montauk Highway should align their driveway to this new intersection. This alternative eliminates all turning movements at the Montauk Highway/William Floyd Parkways intersection.

- Camp Upton Road from Montauk Highway to William Floyd Parkway could be improved and used as an alternative access to William Floyd Parkway for all north and south turning movements. If this alternative is utilized Camp Upton Road should be opened across William Floyd Parkway to McGraw Street. This alternative eliminates all turning

movements at the Montauk Highway/William Floyd Parkways intersection.

In the areas adjacent to and along these alternate access roads, the use of decorative streetlights should be limited to the side of the roadway adjacent to the interior parking areas established at the rear of the uses along the north side and south side of Montauk Highway.

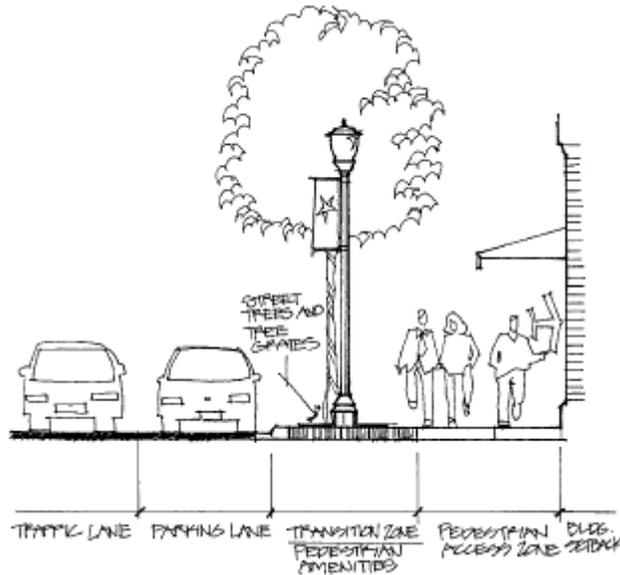
Maintenance of the infrastructure improvements, particularly the landscaping improvements, will necessitate a joint and concerted effort between the Town of Brookhaven, Suffolk County Department of Public Works and private organizations such as the Boy Scouts and Girl Scouts and local civic organizations and Chamber of Commerce. Local landscaping companies can lease or adopt parts (center median) of the landscaped areas to maintain while also advertising their organization with permission granted for small signs identifying their company and/or agency name. A Main Street Business Improvement District should be considered for this corridor in order to fund the cost of providing the necessary maintenance

B. Pedestrian

The transportation improvements, as indicated above, to the Montauk Highway Corridor and other alternative roadways, will improve and enhance the overall pedestrian activity within this area.

The overall Montauk Highway corridor and main street district improvements, as indicated above, will promote, improve and enhance overall pedestrian activity and safety within this area.

The sidewalk is a key element in a main street district as it allows and encourages pedestrian movement. The design and layout of the pedestrian network needs to be considered with the same degree of importance as the street



network. In the main street district, sidewalks are required on both sides of all streets and must connect residential buildings to retail areas, civic buildings, and recreational areas. The width of a sidewalk changes throughout a neighborhood depending upon its location and the type of adjacent land uses.

Sidewalks in retail areas require a minimum width of 10-14 feet to allow for adequate room for pedestrians, as well as trees, lights, outside displays, and awnings. Five foot building setback allows for increased sidewalk width for typical sidewalk uses such as outdoor dining and retail display or softening of the building façade with landscaping. Sidewalks in mixed residential areas typically range from 5-6 feet depending upon the intensity of pedestrian traffic. Sidewalks should be placed to accommodate changes in topography and existing trees and vegetation.

Pedestrian comfort can be enhanced by carefully considering building design, location, setbacks, and orientation. Recommended building design considerations include clustering of buildings, covered walkways, street-level display windows, and avoiding monotonous blank walls.

C. Other Infrastructure.

i. Sanitary.

The formation of a local sewer district is being considered by the Town in order to accommodate the potential design density envisioned within the main street districts. Subdivisions that exceed Article VI sanitary design flow are required by Suffolk County Dept. of Health Services regulations to provide sewage treatment. The Town is considering coordinating such development projects to provide one sewage treatment plan capable of fulfilling the design flow of the subdivision development projects, as well as, provide additional capacity to accommodate the Mastic/Shirley Montauk Highway Corridor. The sewer district, once created, is expected to provide the mechanism to fund and extend the necessary sewers.

ii. Drainage.

Drainage should be handled on-site by leaching pools. A regional recharge basin should be considered to handle storm water runoff from the street network.

iii. Utilities.

All utilities must be located below ground. A concerted effort from all municipalities and utility companies should be undertaken to ensure that during the reconstruction project for Montauk Highway all utilities are placed underground.

Montauk Highway, within the study area, is one of the few areas on Long Island where dual pole lines exist. LIPA/Keyspan owns all the utilities along the south side of Montauk Highway and Verizon owns all the utility poles along the north side. Brookhaven Cable pays those utility companies to co-locate. Verizon has an incomplete network of underground conduit and LIPA has no underground system in the area. Keyspan is advancing gas mains west from the Forge River. Promoting the rerouting of aerial utility lines below grade will significantly improve the quality of the corridor.

In order to accomplish this goal Suffolk County should coordinate the preparation of a utility routing plan as a component of the reconstruction of Montauk Highway. New developments within the corridor should provide the means necessary to relocate underground utilities. Existing business should be encouraged to retrofit to accommodate underground utilities with low interest loan incentives.

iv. Public Parking.

Public parking garage should be considered within each of the main street districts. Parking incentives should be offered to those developers that contribute to the construction of public parking facilities. All parking facilities should be free and open to the public. Access to parking should either direct or indirect to streets having traffic controls.

v. Public Transportation.

The public transportation improvements include: Bus turnouts, bus stops, bus shelters, information kiosks and access to railroad station. A pedestrian access from the rail station to the Shirley Main Street District should be considered.

VII. IMPLEMENTATION

1. Incentives.

In order to achieve the goals of the community vision and the Town as outlined in this document, a series of incentives have been explored and developed to facilitate the transition to viable Main Street Districts.

A. Industrial/Commercial Incentives (485B & Double 485B).

The Industrial/Commercial Incentive Plan was developed to grant tax incentives to spur selective growth and job creation within Suffolk County townships. As per the intent of the incentive plan, the Town of Brookhaven has selected strategic industrial and commercial uses within specific corridors

to support with tax incentives and promote business revitalization. The Town's 2001 amended plan extended the economic incentives to include specific types of business within the Montauk Highway Corridor between Van Buren Street and Mastic Road. The incentives program is intended to provide stimulus for capital improvement projects in the small business sector, that generally do not have access to similar Industrial Development Agency programs. The program requires the commitment of the local school district to support a partial tax exemption on new capital improvements to be phased-out over ten years, after which the commercial building is fully assessed.

The program has broad application within the Study Area, however the Industrial/Commercial Incentives Plan must be amended to extend the Montauk Highway Corridor from William Floyd Parkway to Mastic Road and include McGraw Street and Grand Avenue corridors. Amendment to the Industrial/Commercial Incentive Plan to address the location of specific uses has the potential to provide the greatest mechanism to create the Main Street Districts envisioned.

B. Sanitary Density Transfer Program.

A Town wide acquisition program funded by the Open Space Bond may provide the basis of a sanitary density transfer program designated to facilitate the construction of two and three-story mixed use buildings within the Main Street Districts. Without the benefit of sewer infrastructure, commercial building size and residential density is constrained by Article VI of the Suffolk County Sanitary Code. Article VI regulates the permitted sanitary flow from individual buildings relative to the combined uses, lot size and building gross floor area. The sanitary density transfer program would be available to investors willing to purchase credits or sterilize lands for park purposes within the designated transitional areas along the corridor and provide development within the Main Street Districts consistent with the

design criteria until such time as a regional sewer treatment plant can be realized in the area.

C. Empire Zone.

The Town is developing a program where industrial uses within Main Street Districts would be eligible to relocate to an industrial park in the Empire Zone in order to benefit from tax relief and expansion opportunities. The Town program capitalizes on the Code provision requiring each subdivision to provide suitable property to satisfy recreational requirements. The Town would reserve property within the industrial subdivisions to trade with industrial users in the Main Street Districts. There are a number of industrial uses within the Study Area that would benefit by relocation to the Empire Zone and Economic Development Zone.

D. Payment in lieu of Parking (PILOT).

Downtown mixed-use density goals can be constrained by available parking. Viable Main Street Districts require sufficient parking to conveniently satisfy residential, retail and service requirements. Where planned development within a main street district is constrained by lot size and unable to satisfy requisite parking requirements, a Payment in Lieu of Parking Program can minimize the deficiency. A developer would make a one time PILOT, based on a cost of construction estimate using an average appraised land value, to a dedicated fund. The fund would be managed by the Town or by a Special District and used to establish and maintain municipal public parking areas.

E. Small Business Loans.

This program was developed with Community Development Block Grant funds to provide improvements that will create incentives for small business owners to improve or expand existing business.

Existing small business will be able to participate in one of a number of different loan programs offered by the Community Development Corporation of Long Island (CDC). The Low-Interest Loan Program (LILP) will be available to existing business within the boundaries of Mastic/Shirley corridor that propose development compatible with the goals and objectives of this Plan. In order to qualify for the LILP, business must meet specific use and design criteria in conformance with the Mastic/Shirley Land Use Plan, such as architectural design standards and provisions to accommodate underground utilities. Specifically, all business qualifying for the LILP will retrofit their existing utility connections to eliminate all aerial utilities to the individual buildings.

F. Expedited Review and Approvals.

Main Street District development proposals, in consort with the design criteria presented herein and in compliance with the Town Code provisions, will benefit from expedited review and approval. In an effort to encourage the revitalization of the Main Street Districts, a streamline review and administrative approval will minimize permitting delays. Development proposals eligible for an administrative approval will have the option of phasing on-site improvements pending sanitary infrastructure construction.

G. Relocation.

Present land uses that do not meet the intent and goals of the Main Street District could be relocated to a more appropriate location with the assistance of the Town of Brookhaven. Those uses that are industrial in nature could be

relocated to the Town's EDZ or other locations with the assistance of the Town's Department of Economic Development. Incentives could be offered to these land uses to relocate.

2. Implementation.

It is envisioned by this plan that property owners within the Montauk Highway Corridor will develop or redevelop their properties in accordance with the recommendation and goals contained herein. However, in the event that the goals of this plan are not be realized the Town Of Brookhaven can achive the goals of the plan with the following tools:

A. Special Districts.

Special use or amenity districts are generally formed to provide a collective entity to fund certain public improvements. Sewer district, Sidewalk districts, Street lighting districts and Road Improvement districts have all been used to fund improvements to benefit participants within the district. Business Improvement District are formed to fund improvements, security and on-going maintenance of improvements within district boundaries. The participants benefit economically from the improvements that the collective district supports. The Town has committed to organize, facilitate, supervise and manage, if necessary, the special districts necessary to support revitalization of the Main Street and transition districts.

i. Business Improvement District.

The Town Board is authorized under New York State General Municipal Law to adopt a Business Improvement District. BID may include construction and installation of landscaping, lighting, aesthetic and decorative fixtures, and parking facilities. As well as to provide for the operation and maintenance of any district improvements, opening closing or widening of existing streets, construction of ramps, sidewalks, plazas and pedestrian malls, removal or relocation of utilities, and improvements for persons with disabilities.

B. Public Assembly of Small Lots/Acquisition.

The Town of Brookhaven can assist in the assembly of small lots using their powers of condemnation in order to further the goals of this plan. The assembly of small lots may be necessary to achieve the goals of this plan and to realize a more appropriate planned development along the Montauk Highway Corridor. A substantial private commitment would be necessary to facilitate and partially fund the assembly of small lots and provide infrastructure improvements, such as underground utilities, sewer system and local roadway improvements. The Town Of Brookhaven can also use their powers of condemnation in the future event that the goals of this plan are not being met.

C. Utility Plan.

An underground utility plan should be designed and adopted for the entire corridor with the corporation between the Town of Brookhaven, Suffolk County DPW, LIPA, Verizon, Cablevision and any other public or semi-public utility company. Implementation could be accomplished at the time of the corridor roadway improvements while the roadway construction in underway.

D. Amortization.

The Town Of Brookhaven can develop an amortization plan. In the execution of this plan it is recognized that there are some uses which, due to their very nature, have serious objectionable characteristics and which do not meet the goals and objectives of this plan. Special regulation of these uses may be necessary to ensure that these adverse effects will not contribute to the blighting or downgrading of the surrounding neighborhoods or land uses

An amortization of those uses not consistent with the goals and objectives of this plan would cease doing business within a specific period of time established by the Town Board.

3. Town Code.

The implementation of this plan will require the adoption of new Town Code provisions for a Main Street District land use and zoning. Rezoning of parcels consistent with the Community Vision and the Plan should also be undertaken.

A. Main Street District.

A draft proposed Town Code for a new Main Street District zoning is provided as an addendum.

B. D Residence.

The use of this zoning district could be an asset to those existing residential dwellings along primary alternative access roads.

Addendums

Inventory

MONTAUK HIGHWAY CORRDOR LAND USE INVENTORY

SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 82300 0800 080001	Retail	0	10	2	3	C&S on Montauk, No C&S on Titmus, Street trees on Montauk, No LS
0200 82300 0800 066000	Industrial	Storage	2	0	3	Co C&S, No LS, FY Parking, former Res.
0200 82300 1000 004002	Office	0	1	0	1	C&S, LS, No FY Parking.
0200 82300 1000 004003	Retail/Auto	Office	1	0	1	C&S, LS, Street Trees, Bays to Rear, access to rear.
0200 82300 1000 005000	Auto	Storage	1	0	4	No C&S, No LS, Bays on Montauk
0200 82300 1000 007000 0200 82300 1000 008000	Auto	Storage	1	0	4	No C&S, No LS, Open Air Car Lot
0200 82300 1000 009000 0200 82300 1000 020000	Restaurant	Res. 2-3 Tenants	1	0	2	C&S, LS, No FY Parking, Dirt Parking
0200 82300 1000 021001	Car Wash	0	1	0	2	C&S, Min LS
0200 82400 0200 028000	Office	?	0	1	4	No C&S, No LS, No Parking, Res. Arch.
0200 82400 0200 025000	Retail	Acc Bldg	3	0	3	No C&S, No LS
0200 82400 0200 027001	Retail	0	13	1	1	C&S, LS, 1 Access
0200 82400 0300 055005	Gasoline Filling	C Store	1	0	3	No C&S, No LS
0200 82400 0400 026000	Restaurant	0	1	0	4	No C&S, No LS, No Parking
0200 82400 0400 023000 0200 82400 0400 024000 0200 82400 0400 025000	Auto	Storage	1	0	4	No C&S, No LS, Fenced, No Frontage, No Parking
0200 82400 0400 028000	Industrial	Storage, Detached storage	1	0	4	S poor on Montauk, No C, No C&S on Stuvesant, No LS, No Parking, Bays on Montauk
0200 82400 0400 070000	Retail	Res. in rear, 2-3 tenants	2	0	3	C&S on Montauk, No C&S on Broadway, No LS, Parking on SY
0200 82400 0500 014000	Retail	0	1	0	1	C No S, No C&S on Broadway, Parking on SY, No LS
0200 82400 0500 015000	Retail	2 nd Sty. Res.	1	0	3	No C&S, No LS, Parking on SY
0200 82400 0500 016000	Residential	??	2-3	??	2	No C&S, No LS, Parking in FY
0200 82400 0500 046000 0200 82400 0500 048001	Funeral Home	2 nd Sty, Res.???	1	0	2	No C&S, Min LS, Access on Old Montauk & Washington.
0200 82400 0600 027000	Industrial	Storage	0	0	4	Fenced Storage Yard, No C&S, No LS

0200 82400 0700 013000	Residential	0	1	0	2	Typ. Res.
0200 82400 0700 014001	Industrial	Storage	0	0	4	Fenced Boat Storage Yard, No C&S, No LS
0200 82400 0700 014001	Residential	0	1	0	2	Typ. Res.
SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 82400 0700 014002	Auto	Storage/Depot	1	0	4	NO C&S, No LS, Bays on Montauk, Access to Old Montauk
0200 82400 0800 001000 0200 82400 0800 002002	Civic/Post Office	Storage	1	0	4	C&S, LS, No FY Parking, 2 access
0200 82400 0800 003000	Retail	0	2	0	3	C&S, No LS, No FY Parking
0200 82400 0800 009000	Retail	0	1	0	4	No C&S, No LS, Parking on SY
0200 82400 0800 012000 0200 82400 0800 026000	Retail	2 nd Sty. Res.	5	0	4	No C&S, No LS, Parking in FY & SY
0200 82400 0800 018000	Residential	0	0	0	1	No C&S, Typ. LS.
0200 82400 0800 019000	Retail	SY Res.	0	1	4	No C&S, No LS
0200 82400 0900 001000 0200 82400 0900 004001	Auto	Storage	1	0	4	No C&S, No LS, Parking on Montauk in ROW, Sm. Office
0200 82400 0900 004002	MF Res.	MF Res.	??	??	4	C&S on Montauk, No LS, Parking on Denton & RY
0200 82400 0900 005000	Industrial	0	1	0	4	C&S Poor, No LS, No Parking, Bays on Montauk
0200 82400 0900 011000	Retail	2 nd Res, RY Res.	2	0	4	C&S Poor, No LS, Parking in ROW
0200 82400 0900 012000 0200 82400 0900 017000	Retail	Storage	1	0	3	C&S on Herkimer, No LS, Parking in ROW, Storage in RY
0200 82400 0900 014000	Office	RY Res.	1	0	3	C&S, LS on Prospect (Res.) Parking in ROW
0200 82400 0900 015000	Retail	2 nd Res., RY Res.	1	0	3	C&S, No LS, Parking in ROW & in RY
0200 82400 0900 018000	Auto	Storage/Debris	1	0	4	NO C&S, No LS, No Parking
0200 82400 1000 001000	Retail	0	1	0	3	C&S Poor, Parking in FY & SY, No LS, Former Res.
0200 82400 1000 002000	Retail	0	0	1	4	No C&S, NO LS, No Parking
0200 82400 1000 003000	Retail	Storage	1	0	4	No C&S, No LS, No Paved Parking
0200 82400 1000 004000	Industrial	Storage	0	0	4	Gravel Parking Area & Storage, No C&S, No LS
0200 82400 1000 005000 0200 82400 1000 006000	Industrial	Storage/Debris	0	0	4	Fenced Storage Yard, No C&S, No LS
0200 82400 1000 007000	Retail	2 nd Sty. Res.???	1	0	3	No C&S, No LS, No Parking, Former

						Res.
0200 82400 1000 009000	Retail	2 nd Sty. Res.	1	1	3	No C&S, Min LS, Parking on SY, Former Res.
0200 82400 1000 012000 0200 82400 1000 013001 0200 82400 1000 013002	Civic/Ambulance Co.	0	1	0	4	C&S, LS, Parking in RY
0200 82400 1000 014000	Residential	0	1	0	3	Typ. Res.
0200 82400 1000 015000	Gasoline Filling	Storage	1	0	4	No C&S, No LS, Bays on Montauk
SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 82400 1000 019000	Gasoline Filling	C Store	1	0	3	No C&S, No LS
0200 82400 1000 020000	Auto	0	1	0	3	No C&S, No LS, Bays on Etna & Mastic
0200 82400 1000 025000 0200 82400 1000 026000 0200 82400 1000 027000 0200 82400 1000 028000 0200 82400 1000 029000 0200 82400 1000 030000 0200 82400 1000 031000	Civic/TOB Recreation Center	Play Area	1	0	4	C&S, LS, SY Parking
0200 82400 1000 044001	Commercial Center	0	12	2	3	C&S, Min LS, 3 Access, Parking on all Sides, Movies
0200 82500 0100 019000	MF Res.	0	???	???	2	C&S, LS
0200 82500 0300 001001	Industrial	SF Res./Storage	1	0	4	No C&S, No LS, No Paved Parking
0200 82500 0300 001001	Industrial	Storage	1	0	4	No C&S, No LS, No Parking
0200 85100 0100 001000	Fast Food	Drive Thru/outdoor seating	1	0	2	C&S, Min LS
0200 85100 0100 002000 0200 85100 0100 003000	Civic/Church	0	1	0	1	C&S, LS
0200 85100 0100 004000	Industrial	0	1	0	3	C&S, No LS, On Street Parking
0200 85100 0100 012000	MF Res.	0	???	???	4	No C&S, No LS, Debris
0200 85100 0100 021000	MF Res.	0	2-3	???	4	No C&S, No LS, No Driveway
0200 85100 0100 026000	Residential	0	1	0	3	Typ.
0200 85100 0200 003000	Retail	0	1	0	3	C&S, No LS
0200 85100 0200 008000	Retail	3	0	0	2	C&S, FY LS, Inline Stores
0200 85100 0200 010002	Retail/Office	Office	3	0	3	C&S, Min LS, Parking in SY/Shared
0200 85100 0200 010003	Office	Office	2	1	3	C&S, No LS, Parking in SY/Shared
0200 85100 0200 011000	Retail	Tire sales	4	0	3	C&S, No LS, FY Parking, No Imp on Grand.
0200 85100 0300 003001	Industrial	0	0	0	4	No Imp.
0200 85100 0300 003002	Auto Repair	Storage	1	0	4	LS, No FY Parking
0200 85100 0300 004000	Industrial	Storage	1	0	4	No Imp.
0200 85100 0300 006001	Office	0	5	5	3	C&S, LS, Parking

SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 85100 0300 010001	Industrial	Storage	1	0	3	C&S, Min LS, Aluminum Bldg.
0200 85100 0300 014001	Civic/Church	0	1	0	3	No C&S, No LS
0200 85100 0300 025001	Retail	0	5	0	3	C&S, No LS, RY/SY Parking
0200 85100 0300 027000 0200 85100 0300 032001	Retail	Outdoor Seating	9	2	4	C&S, Min. LS,
0200 85100 0300 033000 0200 85100 0300 034000	Office	0	1	0	2	C&S, No LS, Shared Parking
0200 85100 0300 034000 0200 85100 0300 035000	Retail	0	1	0	3	C&S, No LS, Shared Parking
SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 85100 0300 036000 0200 85100 0300 037000 0200 85100 0300 038000	Retail	0	3	0	3	C&S, FY LS, FY Parking
0200 85100 0300 042000	Retail	Storage	1	0	4	No C&S, No LS, FY Parking
0200 85100 0300 043000	Retail	0	1	0	4	No C&S. No LS, FY & SY Parking,
0200 85100 0300 044000 0200 85100 0300 045000	Retail	0	1	0	2	No C&S No LS, FY Parking
0200 85100 0300 045000	Fast Food	Drive Thru	1	0	1	Interior access
0200 85100 0300 050002, 0200 85100 0300 051000 0200 85100 0300 053004	Commercial Center	Bank W/ Drive thru	30	0	1	3 anchors, 1 U/c, interior access, National Retailers
0200 85100 0300 051000	Residential	0	0	0	0	Historic House
0200 85100 0300 053002	Retail/Auto	Storage	3	1	4	C&S, No LS, Shared access,
0200 85100 0400 017002	Fast Food	0	3	0	1	C&S, LS, 3 Access
0200 85100 0400 018001	Fast Food	Drive Thru	1	0	1	C&S, LS, 2 Access, Interior access
0200 85100 0400 019000	Fast Food	Drive Thru	1	0	1	C&S, LS, 2 Access, Interior access
0200 85100 0400 021002	Office/Real Estate	0	1	0	1	C&S, LS, 2 Access, Interior access, Res.
0200 85100 0400 028001	Fast Food	Drive Thru	1	0	2	C&S, LS, 2 Access
0200 85100 0400 029000	Retail	0	5	2	4	C&S Poor, No LS, Former Res., No C/O
0200 85100 0400 030001	Auto	0	1	0	1	C&S, LS, Overhead doors on Montauk
0200 85100 0500 001000	Auto	Storage	1	0	4	No C&S, No LS, No Parking, No C/o
0200 85100 0500 001300	Industrial	0	0	0	4	Paved Parking
0200 85100 0500 001400	Retail	0	2	0	4	No C&S, No LS, Parking in FY & SY
0200 85100 0500 001500	Office	Res.	1	0	3	No C&S, LS, Parking on Versa
0200 85100 0500 018000	Retail/Office	0	2	0	3	No C&S, No LS, Res
0200 85100 0500 032001	Retail	0	8	4	3	C&S, No LS, 2 Access, CVS
0200 85100 0600 001000	Gasoline Filling	C Store	1	0	4	C, No S, No LS
0200 85100 0600 009001	Retail/Office	0	2	0	3	No C&S, No LS, all Yard Parking
0200 85100 0600 010000	Office	0	1	0	1	C&S, LS, RY

						Parking, 2 access
0200 85200 0100 025000	Retail	0	2	1	3	C&S, No LS, Interior access
0200 85200 0100 026000	Retail	Warehouse	1	0	2	C&S, FY LS, Interior access
0200 85200 0100 050000 0200 85200 0100 052001	Retail/Auto	Auto	2	0	4	No C&S, No LS, FY Parking, Bays to rear
0200 85200 0100 053000 0200 85200 0100 054000	Retail/Office	0	2	2	4	No C&S, No LS
0200 85200 0100 055000 0200 85200 0100 056000 0200 85200 0100 038000	Retail	0	1	1	4	No C&S, No LS
SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 85200 0100 057000	Retail	Storage	1	0	4	No C&S, No LS
0200 85200 0100 058000 0200 85200 0100 060001	Industrial	Storage	1	0	4	No C&S, No LS, No Paved Parking
0200 85200 0100 061000	Retail	0	3	0	3	No C&S, No LS
0200 85200 0100 086000	Vacant/Clea red	0	0	0	0	None
0200 85200 0100 063001	Gasoline Filling	Repair/C Store	1	0	3	No C&S, No LS, Bays on Montauk, Junk Cars
0200 85200 0100 064000	Retail	0	1	0	3	No C&S, No LS, FY Parking, Acc. Bldg.
0200 85200 0100 066001	Office 2 story	0	4	0	1	C&S, LS
0200 85200 0100 067000	Office	2 - Res. in Rear	1	0	2	No C&S, No LS, No Parking
0200 85200 0100 070001 0200 85200 0100 071000	Car Wash	0	1	0	1	C&S, Min LS, Bays face Montauk
0200 85200 0100 075001	Retail	0	4	0	2	C&S, No LS, Parking in FY
0200 85200 0100 079000 0200 85200 0100 080000 0200 85200 0100 081000	Residential	0	1	0	3	Frontage on Hoover
0200 85200 0200 001000	Auto	Storage	1	0	2	C&S, FY LS, Bays on Montauk
0200 85200 0200 006001	Retail	Retail	2	0	2	No C&S, No LS, Access from Haven
0200 85200 0200 007000	Retail/Office	0	4	0	3	No C&S, No LS, Parking on Haven SY
0200 85200 0200 008000	Auto	Storage	1	0	4	No C&S, No LS, Bays on Montauk, 2 Access
0200 85200 0300 005001	Retail	0	1	0	1	C&S, LS, no Montauk access
0200 85200 0300 006000	Auto	Storage	1	0	4	No Imp.
0200 85200 0300 007000	MF Res.	Debris	2-3	??	4	No Imp.
0200 85200 0300 008000	Auto	Storage	1	0	4	No C&S, No LS, Bays on Montauk, 2 access
0200 85200 0300 009001	Retail	0	2	0	3	C&S, No LS, FY Parking
0200 85200 0400 001000	Restaurant	Res. 1-2	1	1	3	No C&S, No

		Tenants				Parking, Former Res.
0200 85200 0400 006000	Retail	0	1	0	3	C&S, No LS, 2 Access
0200 85200 0400 007000	Residential	0	1	0	3	No C&S, Typ. Res. LS, Access on Cedar
0200 85200 0400 015000	Retail	Storage	2	0	4	No C&S, No LS, No Parking, Former Res.
0200 85200 0400 020000	Auto	Detached Res. 2-3 Tenants	1	0	4	No C&S, No LS, Bays on Hawthorne, Parking in FY
0200 85200 0500 002001	Auto	2 nd Office RY Depot	1	0	4	No C&S, No LS, Bays on Montauk, Parking in FY
0200 85200 0500 003001 0200 85200 0500 004001	Auto/Industrial	Storage	2	0	4	2 Res. Buildings, No C&S, No LS, Parking in FY
0200 85200 0500 009002	Retail	0	1	0	1	C&S, Min LS
0200 85200 0500 011002 0200 85200 0500 012000	Industrial	Storage	2	0	4	No C&S, No LS, Bays on Montauk
SCTM #	Use	Add. Use	Tenants	Vacancies	Condition	Improvements
0200 85200 0500 015000	Office	0	1	0	4	No C&S, No LS
0200 85200 0500 017002 0200 85200 0500 017003	Industrial	0	1	0	4	No C&S, Street Trees, No LS, 3 Access

LEGEND:

Bays = BAY DOORS
 C = CURB
 C Store = CONVENIENCE STORE
 FY = FRONT YARD
 LS = LANDSCAPING
 Min. = MINIMAL
 Res. = RESIDENCE
 RY = REAR YARD
 S = SIDEWALK
 Storage = OUTDOOR STORAGE
 Sty. = STORY
 SY = SIDE YARD
 Ten. = TENANT
 Typ. = TYPICAL

CONDITION:

1 = Excellent
 2 = Good
 3 = Fair
 4 = Poor

MONTAUK HIGHWAY CORRDOR LAND USE AREA

USE	SQUARE FEET
Retail	174,224 sq. ft.
Commercial Center	315,180 sq. ft.
Office	41,086 sq. ft.
Fast Food	15,582 sq. ft.
Gasoline Filling	10,036 sq. ft.
Industrial	67,360 sq. ft.
Residential	21,062 sq. ft.
Mixed	5,281 sq. ft.
Civic	3,813 sq. ft.

NOTE: All information obtained from assessment records found in the Town Of Brookhaven Assessor's Office.

Proposed Rezoning Parcels

REFERENCE #	TAX MAP #	FROM ZONE	TO ZONE
MSD1	0200 78600 0400 021000	A1	D
MSD2	0200 82200 0400 001000	A1	D
MSD3	0200 82200 0400 002000	A1	D
MSD4	0200 82200 0400 003001	A1	D
MSD5	0200 82200 0400 003002	A1	D
MSD6	0200 82200 0400 004000	A1	D
MSD7	0200 82200 0400 005000	A1	D
MSD8	0200 82200 0400 006000	A1	D
MSD9	0200 82200 0400 007000	A1	D
MSD10	0200 82200 0400 008000	A1	D
MSD11	0200 82200 0400 009000	A1	D
MSD12	0200 82200 0400 010000	A1	D
MSD13	0200 82200 0400 011000	A1	D
MSD14	0200 82200 0400 012000	A1	D
MSD15	0200 82200 0400 013000	A1	D
MSD16	0200 82200 0400 014001	A1	D
MSD17	0200 82200 0400 014004	A1	D
MSD18	0200 82200 0400 015000	A1	D
MSD19	0200 82300 0100 024000	A1	D
MSD20	0200 82300 0600 005000	A1	D
MSD21	0200 82300 0600 011000	A1	D
MSD22	0200 82300 0600 021000	A1	D
MSD23	0200 82300 0600 023001	A1	D
MSD24	0200 82300 0600 024000	A1	D
MSD25	0200 82300 0600 025000	A1	D
MSD26	0200 82300 0600 026000	A1	D
MSD27	0200 82300 0600 027000	A1	D
MSD28	0200 82300 0600 028003	A1	D
MSD29	0200 82300 0600 029000	A1	D
MSD30	0200 82300 0600 030000	A1	D
MSD31	0200 82300 0600 031000	A1	D
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Vision Report



**Appendix
Vision Document**

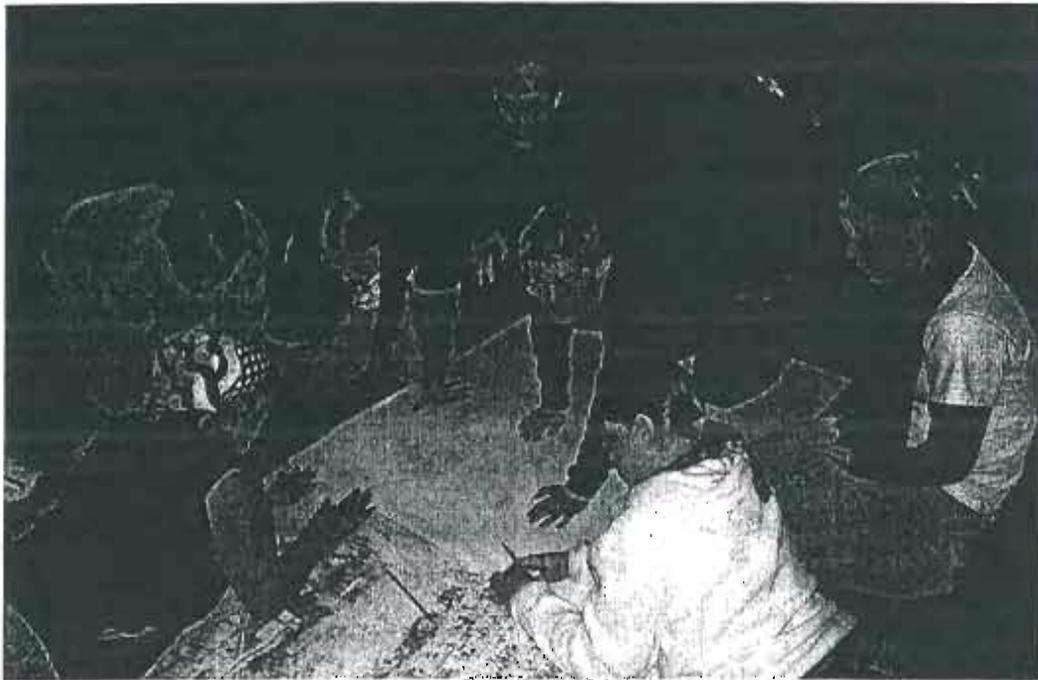


MONTAUK HIGHWAY PROJECT

VISION REPORT

TOWN OF BROOKHAVEN, NEW YORK

APRIL, 2002



Sponsored by:

*William Floyd Community Summit, The Chamber of Commerce of the Mastics
& Shirley, Town of Brookhaven, Suffolk County*

Submitted by:

ADL III Architecture in association with Vision Long Island

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The contents of this report represent the knowledge, experience and expertise of the citizens and authors in providing ideas and concepts to improve safety, access, mobility and livability through streetscaping, transit, walking, bicycling and other traffic management strategies. This report does not constitute a standard, specification, or regulation; it is not intended to be used as a basis for establishing civil liability. The decision to use a particular measure should be made on the basis of an engineering study of the location. This report is not a substitute for sound engineering judgment. Adherence to the principles found in this report can lead to an overall improvement in neighborhood and community livability and traffic safety.

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Shirley Ambulance Company, Elite Transportation, Inga
Norblatt, Mike Cordaro, Joseph Pergola, George Lemay,
John Raptis, Paul Micallef, Manny Larios, Carole LaRocca,
Amy Illardo and Floyd in Focus.

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Part One

Introduction, Summary and Existing Conditions



Montauk Highway. Over the years Montauk Highway has changed from a historical community hub to its present incarnation as a busy thoroughfare with traffic added volume, speed and confusion. Increasing traffic and parking demands have brought an abundance of signs, graying parking lots and an aging infrastructure. Many new buildings in the area have been built as a result of reactive planning, rather than a proactive community vision. The Montauk Highway of the future is to be built upon a developed community vision.

Introduction

In early February 2002, the Mastics and Shirley residents took part in a series of walking audits, workshops, training sessions, design studios and public presentations. The work focused on developing town streetscape plans for Montauk Highway, including both the street itself and the land buffering it on either side.

This report summarizes four days of work, fun, and creativity by more than 500 people who gathered together breathing new life, color, safety and efficiency into this growing community. As they worked together to envision a new streetscape, participants were asked to consider all land uses, transportation modes, recreation, shopping, entertainment, residential, housing and other service needs in the study area.

ADL III Architecture, Inc. led a design team that conducted interviews, observations and facilitated a series of ten events with a spectrum of Town of Brookhaven residents, property owners, retailers, staff, elected officials and other stakeholders.

Among the issues raised and discussed:

- Creation of a public plaza, with a town statue benches, gardens, decorative pavement, green space and drinking fountains.
- Definition of the Mastics and Shirley's "100% location", i.e. a place where many desirable elements converge. Workshop participants specifically cited the need for a social public gathering space and teen areas.
- Improved efficiency and performance of Montauk Highway for moving area and local traffic.
- Improved access and safety of motorists, pedestrians and bicyclists. Bike lanes, wide sidewalks.
- Elimination of ugly graying space. It was felt that the addition of more green space would create more pride in the town.
- Improved pedestrian access to key retail properties.
- Creation of improved ways to cross Montauk Highway and other principle roadways.
- Reduced travel speed to more appropriate and safe speeds of 30 mph along Montauk Highway.
- Addition of improved traffic management tools and gateways, such as medians and roundabouts
- Aesthetic improvements to include attractive street lamps, trees.
- Aesthetic, functional alleys linking Montauk Highway commercial properties to a town plaza and parking.
- Elimination of unsightly utilities and other poorly located objects.
- Cleanliness and maintenance, including commercial code enforcement.

Community Driven Process. Ten separate events packed into a four-day public process allowed over 500 residents to create a vision for Montauk Highway. Reams of dreams and talent emerged from tables. Youth, adults, and area professionals added essential points and details to the final plan.

Vision drives design. Towns across America are learning that with a coordinated, locally developed vision former impossible dreams become reality. New energy and new commitment result from a town that unifies and builds its dreams together. Former negative energy is transformed into positive, collective community-building spirit. In the end residents are proud, productive and happy.

Questions: Can the slowly evolving Mastics and Shirley community better embrace its future through planned growth? Can the driving, walking, bicycling and transit experiences be built and changed in a way that embrace the history and philosophy of settlers, while building in missing elements of a community life? In recent years, some buildings, traffic management decisions and land uses in the downtown have resulted in unhappy places and dissatisfied residents.

Town Making Process. Nationwide, new and “as old as the hills” methods of town and street design are offering citizens exciting new prospects for building lively, yet quiet, pleasant places to assemble, shop, be entertained and associate with friends and strangers. A new process using highly interactive workshops is rebuilding trust and partnerships, allowing many new formerly forbidden dreams to be achieved.

Place Making. Collectively the caretakers and stakeholders of the downtown and adjoining neighborhoods seek a missing sense of place. This place should offer the Mastics and Shirley community a mix of public social life, vibrancy, activity, identity and purpose. Although Smith Point County Park, hosts some wonderful seasonal events and provides much needed recreational space in the area, it is not located within a comfortable walking distance of the commercial district. Today there is no central, organized, attractive place to hold festivals, or to linger and relax in a retail space away from the noise of traffic. Young children and teens lack a place where they feel comfortable and can be observed and cared for. The commercial district is not currently friendly to people with disabilities and older seniors.

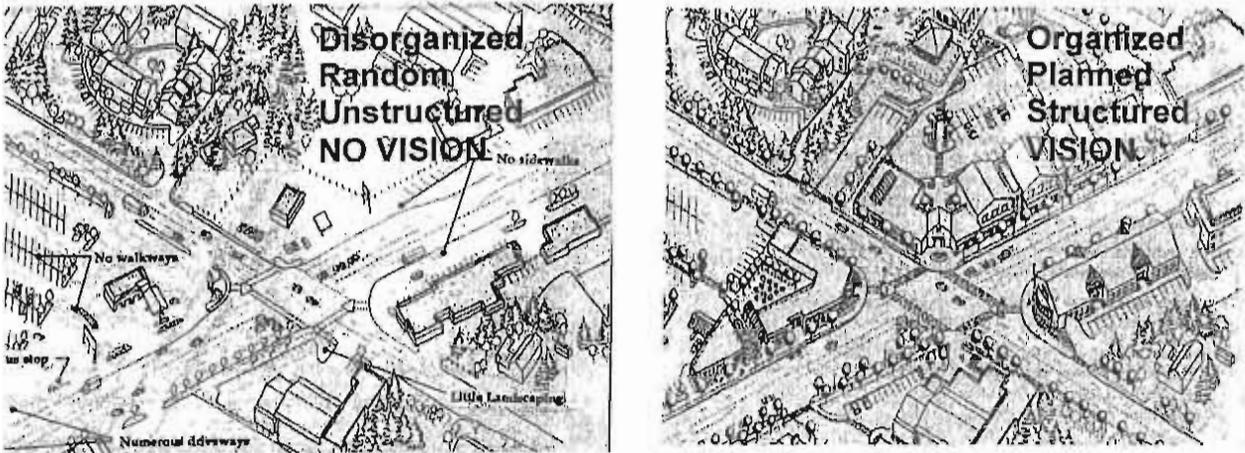
Auto Traffic. Auto traffic and the need for parking dominate current space use. This saturation will be addressed through traffic management in the area, but no amount of ‘tweaking’ will reduce current traffic congestion on Montauk Highway. Alternatives to both driving space and travel modes in the corridor are badly needed.

Proper speed is dictated by a combination of improved geometrics, operations, tree canopies and building types. Motorist/pedestrian conflicts in higher speed areas will be addressed. Innovative techniques in street making will add more green space, reduce impervious surfaces and provide more planter strips, medians with turning pockets, walkways, and sitting places.

Designers will build vistas terminating in attractive roundabouts, and will give more emphasis to moving people with bikes and foot power. Pedestrians, bicycles and transit will make up a higher percentage of travel modes.

Parking and driving will be easier and more convenient. “Way finding” (i.e. the ability to get from one location to another quickly) and locating parking lots will be easier, street crossings will be simplified and better channeled. Walking from existing neighborhood streets to the town center, civic buildings and commerce will be more secure, fun and rewarding.

Vision vs. Reality. Today the visitor in a car sees graying, poorly landscaped parking lots, heavily congested roads, utility poles, and ponding water. Montauk Highway and other principal roadways suffer from the lack of planning and vision that exemplify much of the past forty years. Residents and workshop participants were unanimous in their priorities. The great majority seeks a quieter village atmosphere with places of beauty that are restful, comfortable, welcoming and serene.



Today, walking is unsafe, uncomfortable and unwelcoming. There is a lack of safe walking or bicycling access to crossing Montauk Highway. Waiting for transit is uncomfortable in most places. Shade is limited. There is no clear sense of place making, uniformity or identity.

Most noticeably, traffic speeds through places important for social and civic events. For the purpose of this study ADL III Architecture concentrated on methods to create place, making use of improved land development, bringing streets into a healthy, appealing form, keeping traffic in motion, and generating a clean, attractive, walkable, welcoming environment.

Montauk Highway To Create Missing Town Potential. The vision most people have for Montauk Highway is uniform. Each segment of the community seeks a street and public area that completes a sense of village life. Montauk Highway will transform driving into a safe, orderly, steady progression of movement through the community.

Public Consensus: Workshop participants expressed a remarkable degree of public consensus on key topics, and provided an effective forum to explore other planning and design ideas that should help shape the future development of a village center. The William Floyd Community Summit, and other sponsors of this workshop would like to see this public consensus achieving process refined and applied to this and many future concerns such as land use, transportation and other community and natural habitat development or preservation issues.

The process identified missing elements transforming the Montauk Highway area into a more attractive, functional, safe and enjoyable place to work, shop, be entertained and live. Developed properly, the area will function as a center of public activity, hosting new public space, businesses, services and entertainment and providing better linkages, sidewalks, parks and streetscapes. By providing new opportunities for interaction, the changes will bring more people to the area.

Study Area

The study area includes all land inside the following boundaries: all of Montauk Highway (*both the street itself, the land buffering it on either side*) from William Floyd Parkway to the West and Barnes Road to the East.

Participants were asked to include all land use, linkages, transportation, recreation, shopping, entertainment, residential and other service needs in this study.

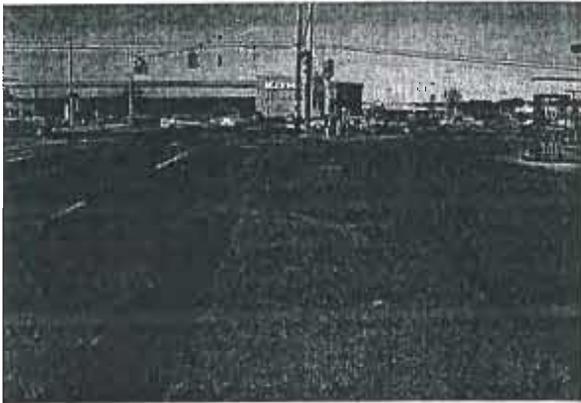
A Visual Survey of Existing Conditions on Montauk Highway



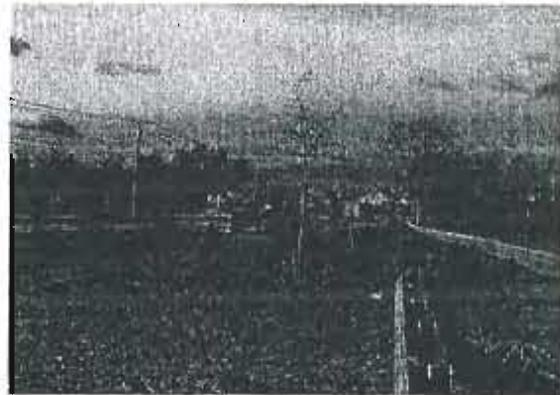
Sidewalks are often used for parking



Community residents seek bike lanes and lower auto speed



Very wide roads with no landscaping or beautification



Poorly orchestrated parking and lack of green space



Environmental assets not adequately accessed



Active businesses not unified

Part Two

Principles And Process

Public Process Develops Ownership

The plan started without preconceived notions other than that consensus would be built from a common developed vision. The ten separate events gave people many ways to contribute, learn and network. This same process can also be applied to even tougher community development decisions.

Principles

The principles of walkable communities were reflected in training presentations conducted during the event. These are general principles that are pertinent to the development of walkable places in many communities.

The principles for creating walkable communities are not new. Many older cities are excellent examples of planning and design that encourage walking. Many people love to visit great cities such as New York City, San Francisco, and Boston because they are vibrant, joyful, and alive with people. These exciting communities experience high levels of traffic congestion, but they also enjoy high levels of pedestrian use.

But somewhere along the path to the present, accommodating vehicles became a priority so demanding that the new ways of building place evolved. Pedestrians, if accommodated at all, were treated as impediments to traffic flow. Streets grew wider, with more lanes. Access to goods and services without a vehicle became difficult, if not impossible.

In many cases, beautiful old neighborhoods were pierced by wide, sterile strips of asphalt. People moved. Neighborhoods deteriorated. The tax base eroded and the street no longer paid for itself. Fewer people walked. Pride of place disappeared.

Now communities are seeking a different emphasis. The lesson lies in the past: properly designed streets foster community. The impact of traffic and street design on community values is being evaluated. Communities want issues of public safety and security, community cohesion, environmental compatibility, preservation of historic and cultural resources and visual quality considered when streets are developed.



Context Sensitive Design

The engineering community is responding by evaluating street designs within the context of the needs of communities. This approach has been labeled 'Context Sensitive Design' by many professionals, and is promoted by federal agencies and many states. The term defines an approach to transportation decision-making that is:

- **Visionary - How will things look and work in the future**
- **Considerate of community values**
- **Addresses the interaction between highway and adjacent areas**

Context sensitive design is not a new set of street standards to be applied in certain situations. It is a mindset that encourages the design of transportation facilities as places for people that are representative of the values of the community, and that are in harmony with the surrounding landscape and land use. The flexibility to achieve this people-friendly design exists already within the standards and guidance used by engineers.

"Pedestrians are the lifeblood of our urban areas, especially in the downtown and other retail areas. In general, the most successful shopping sections are those that provide the most comfort and pleasure for pedestrians."

American Association of State Highway Officials, A Policy on Geometric Design of Highways and Streets, 1994

The pages that follow discuss the principles of planning and designing for walkable communities. It is through careful selection of the elements of the street and attention to detail that truly wonderful, walkable places are created.



Principle #1 - Walking Distance

In a walkable community, a range of residential, leisure, commercial and educational activities focus around a one-quarter mile radius. Most people will walk up to ¼ mile for errands, visiting or other purposes. In attractive areas, walking distances will extend up to half mile and more. Land use must be managed to create a variety of attractions in one place, i.e. village centers. To keep these spaces alive, vital and fun, it is essential to mix activities. Often many activities are combined in one building or block.

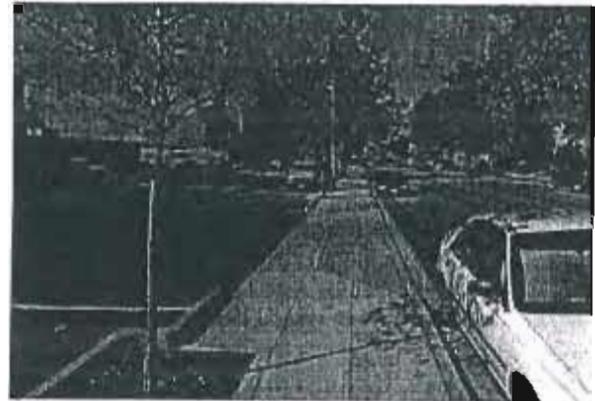


Principle #2 - Reduced Speed

Roadway speeds must be moderate (20-25 mph are most common). Landscaping (street trees) and medians, combined with bike lanes and on-street parking, help convert formerly wide, multi-lane roads into more tame, people-focused places. Wide sidewalks in downtown areas are essential to support transit and walking. These treatments effectively convert high-speed roadways into quieter places, supportive of walking and bicycling. Curbs, gutters and drainage facilities are not impacted with these changes. On two-lane roads speeds are set by prudent, rather than impulsive drivers. For additional detail see ([www.walkable.org/Road Diets](http://www.walkable.org/RoadDiets)).

Principle #3 – Landscaped Edges and Walkways

In village areas, landscaping is needed on both sides of each sidewalk to buffer walkways from streets and parking areas. In light shopping areas, sidewalk widths as little as 8-10 feet are common. A width of 5 feet is an absolute minimum for anything but solo walking. Six feet is required when walkways are attached to curbs. Shopping districts require sidewalk widths of 8-16 feet or more for anticipated volumes.



Sidewalks are needed on both sides of streets. Sidewalks are most often set back from roadways 6 to 20 feet in suburban areas. Parking lots should be buffered and hidden using attractive visual landscaped screens. Fences should be minimized or eliminated. The combination of low shrubs and raised borders screens large gray areas of parking lot asphalt, and enhances security. Low shrubs and open landscaping are important in maintaining space with no hiding places.

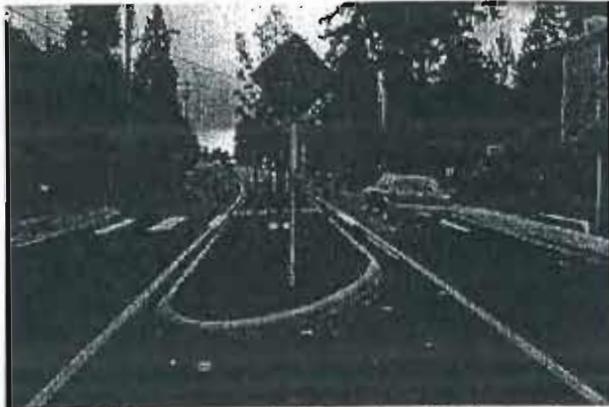


Principle #4 – Well-Connected, Easy Crossings

Walkable neighborhoods or urban centers require high levels of connectivity and ease in crossing streets safely. For variety, route choice and ease of travel, pedestrians need many connections. Pedestrians will walk up to 150 feet out of the way to find good points to cross main streets. Therefore, organized crossings are needed each 300 feet along main street environments. Medians and curb extensions help define safe, visible and inviting crossings that will be well used.

People feel most comfortable walking when buffered from both the street and parking lots. Measure your own comfort in walking along these two walks (pictured above). Note these walkways are across the street from one another. One sidewalk gets more use than the other. Smart retailers install well-designed walks.

When main streets are overly wide with high speed/volume traffic, refuge islands and medians are needed to assist pedestrian crossings. Medians can be orchestrated in many locations to help reduce the number of conflicts that pedestrians must identify and avoid. Pedestrians should not be required to stand unprotected in center lanes to wait for gaps in traffic.



When driveways and demand of left turning vehicles is high, then refuge island design can be applied

Off-street parking can be converted to parallel or diagonal parking (on side streets). Improved use of space benefits retailers and developers by creating more convenient parking.

Principle #5 - Design for Basic Human Needs

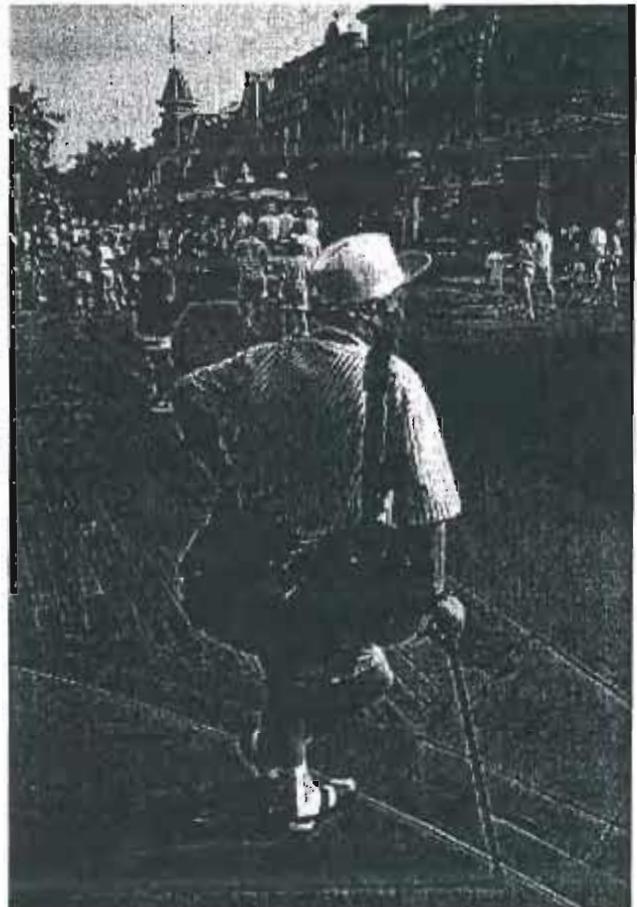
Successful places of any size or scale address a compliment of five basic human needs. These five elements should be considered at both the micro- and macro-levels. Whether trying to market a barbershop, restaurant, movie theatre, theme park, transit delivery system, neighborhood or commercial district, all human beings respond to high levels of the following elements:

1. Security
2. Convenience
3. Efficiency
4. Comfort
5. Welcome

Security

Designers must provide for both real and perceived security. Many “eyes on the street” are needed. These “eyes” are provided from nearby buildings, by abundance of human activity, through open landscaping and other design. Typical blank walls and building setbacks weaken the feeling of security.

The feeling of personal security takes many forms. The most important ingredient is the presence of many people. Noisy, congested, fume-filled streets lead to an erosion of the presence of people and enjoyable place. Street design to induce low speeds and steady movement of traffic is critical to interactive place.



Convenience

To be useful and functional, streets should provide access to most of the commercial needs of community residents and visitors. Once cars are parked, there should be no need to climb back into them for more shopping or other errands.

Most people working in retail shops should be welcomed to own residences in the immediate neighborhood and walk or ride bicycles to their work places. The original town center has historic and compact form, allowing high levels of walking and bicycling convenience. In recent years many people have moved from the central town area, creating long and inefficient patterns of travel.

Efficiency

People seek ways to make efficient use of their time and money. Affordable pricing hinges on maximizing use of land, buildings and place. Wide spacing and full separation between land uses, poorly located parking and transit stops and other elements of isolation lead to low efficiency. Walking efficiency is fully dependent on short distances between multiple vendors and services. More compact land use achieves missing efficiency. This efficiency can be clustered in pedestrian pockets, tight combinations of public space, shops, work places and residential areas.

Comfort

Comfort for the motoring public is largely met in climate controlled, lumbar supported, sound dampened car environments. Goods are carried in trunks of cars. Trash is temporarily stored in litter-bags, and restrooms are often toe-upon-accelerator away. In sharp contrast, pedestrians find their needs met through shady streets with wind breaks, convenient placement of stores, green buffers to the speed of cars and emptiness of parking lots, occasional benches for sitting, litter canisters, and clean, conveniently placed rest rooms, water fountains, gardens and other quality details of built urban environments.



Successful streets attract stores with large windows facing the street, people gathered around tables, and others standing on street corners. Security is based on having many others around, and lots of windows and eyes on the street.

When walking was the primary means of mobility, towns were laid out with compact, convenient form. Only with the advent of autos did city planners forget the importance of keeping intact walking scale villages.



Efficiency is achieved when buildings supply space for the many and varied goods and services of a fully functional village center. Housing, stores, post office, bank, outdoor seating, transit and other elements are all present in a compact village center. This mixed-use building at Orenco Station in Portland, Oregon, meets many of the needs of residents and visitors. The variety of stores keeps the street lively. Many eyes on the street add to the security.

Welcome

Welcome is achieved when people are invited to attractive, relaxing places through close attention to detail. Gateway entries, colorful buildings and awnings, well kept walkways, public art, and nicely landscaped streets are building blocks of welcome. Festive activities, diversity, choice, quality smells and sounds, low traffic speeds, clean, well-kept environments add to welcome. Screening utility vaults, dumpsters, parking lots and other elements that offend the senses are also important factors in welcoming people to the area.

For decades pedestrian-scale street lamps were viewed by road building agencies as frill and fluff. Today, smart communities work to add benches, low-level lighting, bike racks, decorative features and other amenities to build welcoming environments. The estimated 10-15% increase in cost is repaid through increased business, tourism, pride and community spirit. Senses of place and belonging are only achieved by adding the extra cost to projects. Streets should be viewed as living rooms or front porches. Neglect the furniture, decorative trim and the house loses much of its value. The same is true of main streets



Sag Harbor



Greenport

THE WORKSHOP PROCESS

Day One –

Six activities were held. Each allowed different folks to share their insights, hopes, dreams, ideas, concerns and issues. Starting Friday morning where focus group sessions were held. One group addressed issues faced by retailers. Another focus group addressed issues faced by the implementers of the completed vision -- the Town, County, State and Federal Government land use policies.

Focus groups impart specialized knowledge and bring out concepts that might otherwise remain hidden until the end of the workshops. The comments helped the design team focus, validate and listen to the public at large at meetings later in the evening.



PRIORITIES

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Nice Landscaping	16
Nicer Stores	14
Lamp posts and benches	13

Day Two –

Four walking audits were conducted, one by a youth group, and three with adults. Participants talked about what worked for them in different parts of the study area, and what did not work. Some groups did role playing, looking through the eyes of an eight year old child, an 80-year-old senior, and a person with mobility limitations.

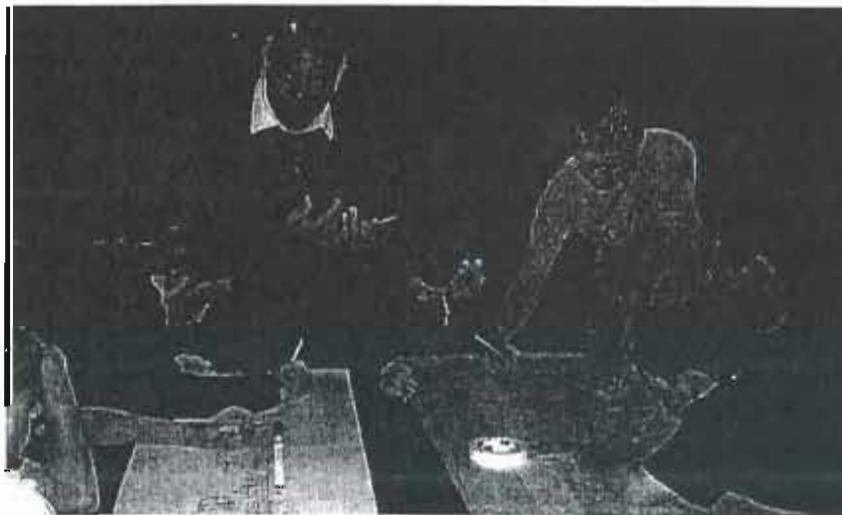
Following an indoor morning critique of street designs shown on an LCD projector, the nearly 200 participants took a lunch break.

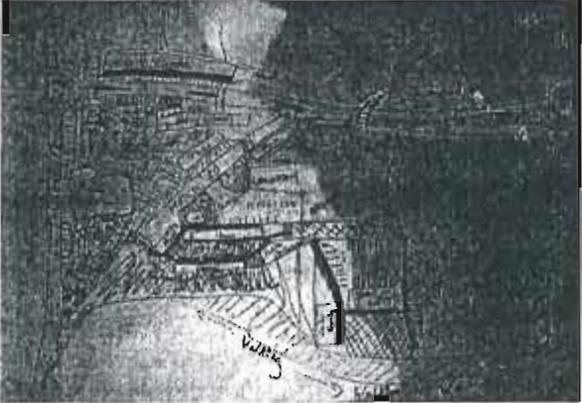
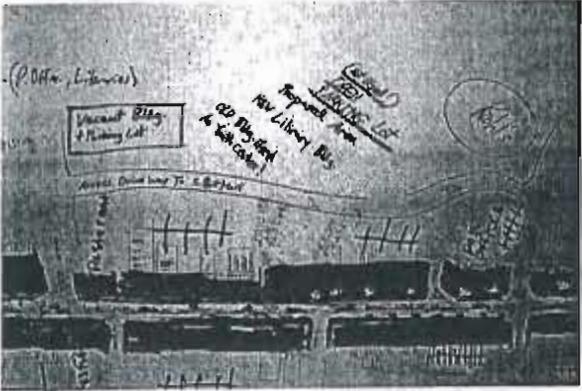
Following the lunch break, adult and youth participants split into tables that had a mix of interests, from residents to retailers to investors, developers, political leaders and others.

At the end of the brainstorming session, each group presented their ideas to the others. The youth presented first. Next the adult participants presented their findings from nearly two hours of table design.



Argue with your pens, not your mouths

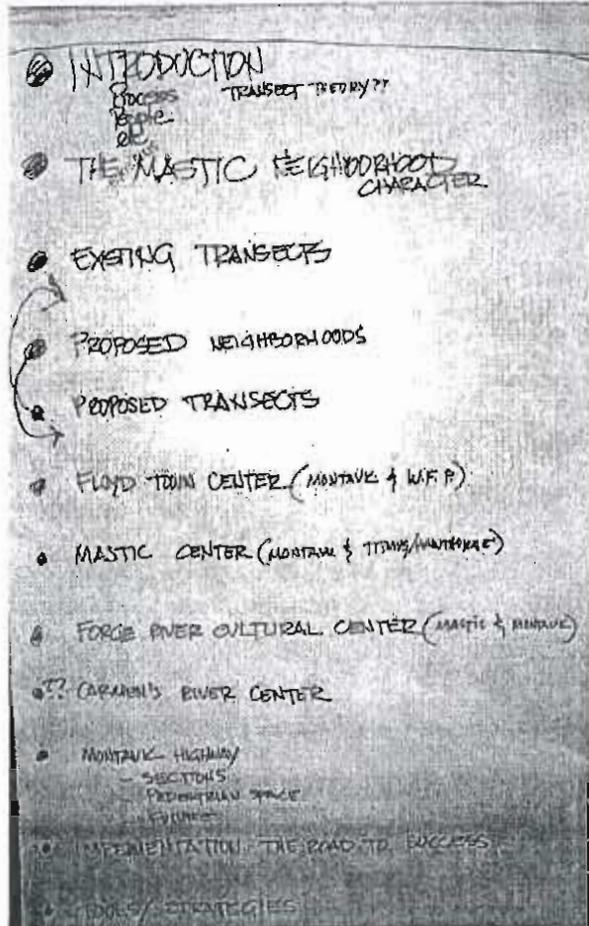




Day Three – Professional Design

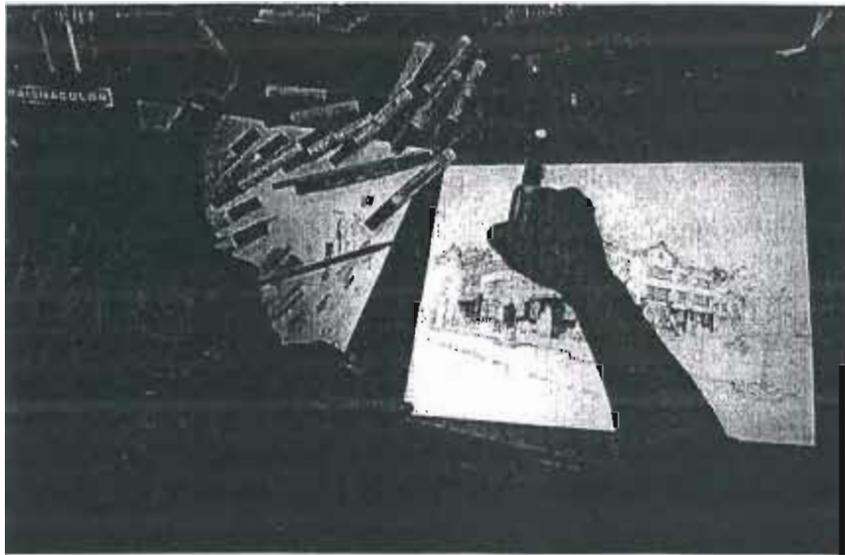
Workers assembled fifteen table designs on three large walls. Town Planner Scott Lewendon condensed the overall concepts from the design table sessions. By adding his knowledge of successful Town center and community design, Scott and architect Alex Latham were able to sketch out a general concept for the new Montauk highway corridor.

Eleven design professionals (see credit list) from the Long Island and Brookhaven area prepared plan views, renderings and additional visual support for the plan.



Part Three

Solutions



RECOMMENDATIONS



Recommendations for the overall corridor will be divided into the five categories including three neighborhood centers:

1. **Grand Avenue**
2. **Hawthorne Center**
3. **Twin Ponds**
4. **Street Sections**
5. **General Recommendations.**

1. GRAND AVENUE

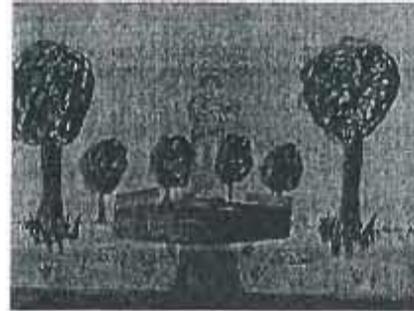
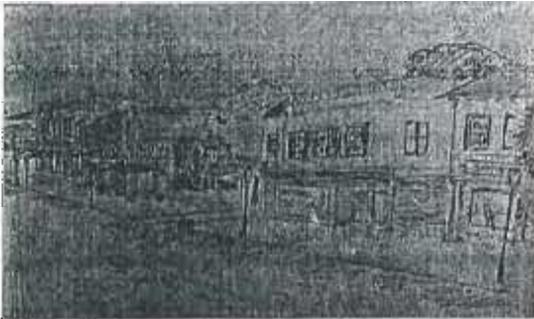


The character of the intersection of William Floyd Parkway and Montauk Highway is an example of the consummate suburban sprawl intersection designed to make an environment as friendly to the automobile as possible. Since the northeast corner of this intersection is undeveloped, the community had focused their interest in developing this parcel to:

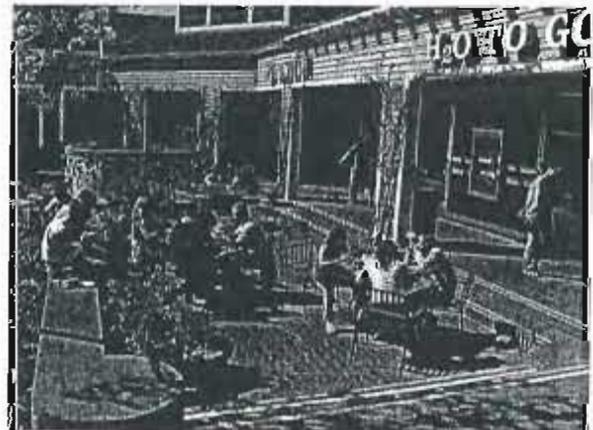
- a) Fill in the open space with a designed commercial neighborhood center, which emulates a downtown feel that would be very walkable.
- b) The center of this new neighborhood would be Grand Avenue located equidistant from William Floyd Parkway and Southport Shopping center.
- c) The northern east/west avenue of the center would be McGraw, and this street would be developed to the north in the later phases of the build out of this commercial neighborhood.

The design of this development would be comprised of two to three story buildings fronting the street with parking behind the buildings. With buildings fronting the street, twelve foot wide sidewalks, street trees, on street parking, retail uses at street level, and offices and potential apartments on the second and third floors, the vitality of shopping and socializing in a downtown atmosphere would unsurpassed in the area. The schematic drawings of this area are illustrative of how well integrated this type of development can be designed into the existing street fabric and into the existing suburban fabric that exists in the Mastic/Shirley area.

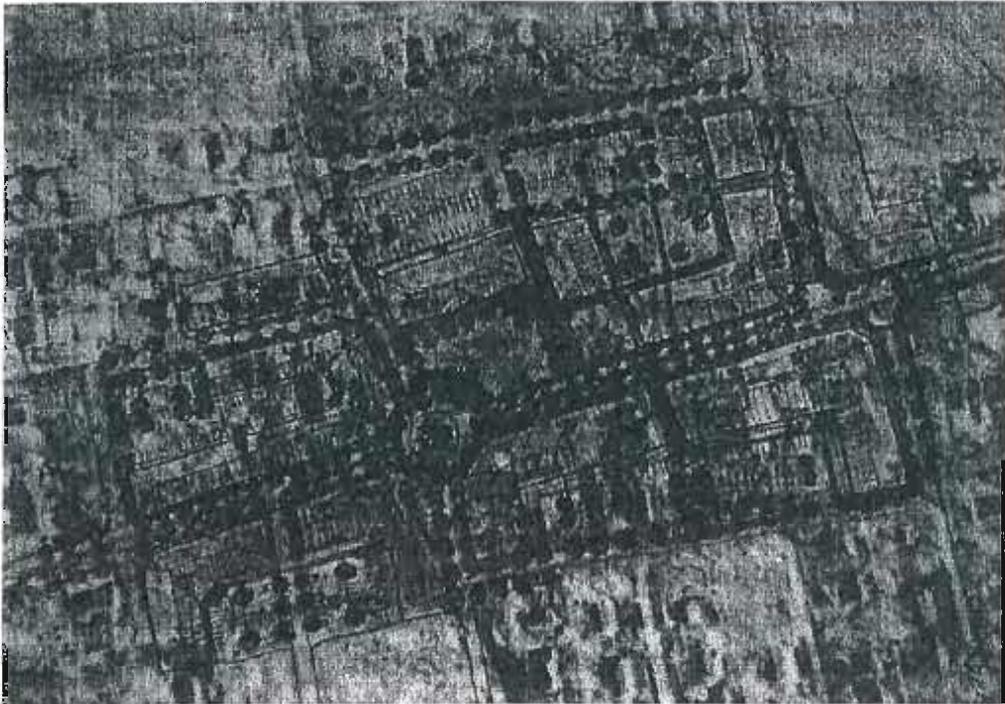
McGraw St. and Grand Ave. would become "Main Streets" within this neighborhood. It is strongly urged that the eastern terminus of McGraw be developed as an enhanced gateway into the existing Southport Shopping Center.



Future development to the area north of McGraw and south of Sunrise Highway will be less conducive for single family residential and more appropriate for additional retail & office uses and also apartment housing. The street grid in this area would be continued north and connected to a future service road for Sunrise Highway.

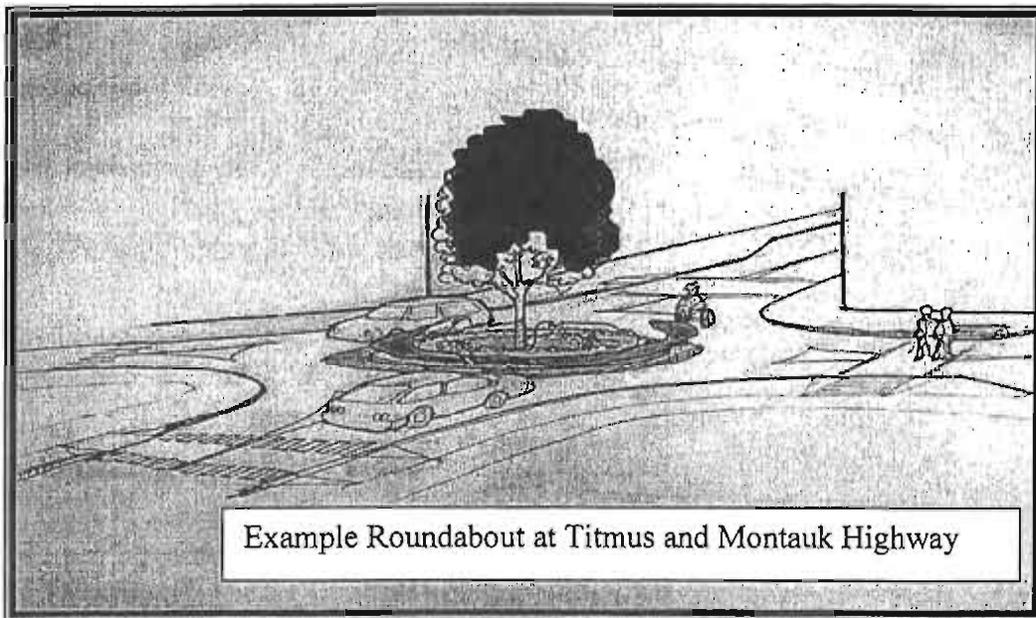


2. HAWTHORNE CENTER



One of the major aspects of the "Hawthorne Center Neighborhood" has to do with the development of a new north/south travel axis along Titmus Drive (North of 27A) and Hawthorne Street (South of 27A). Currently, Titmus Drive crosses over Sunrise Highway and connects to Hawthorne Street. Southerly access to the peninsula is blocked by the east/west Long Island Railroad. It is strongly proposed to allow Hawthorne Street to cross the railroad tracks over with a bridge or preferably under with a trestle.

The major intersection of Hawthorne Center would then be at Route 27A at Titmus Drive and Hawthorne Street. A roundabout should be developed at this intersection as a center point (figuratively and literally) of this neighborhood.



Example Roundabout at Titmus and Montauk Highway

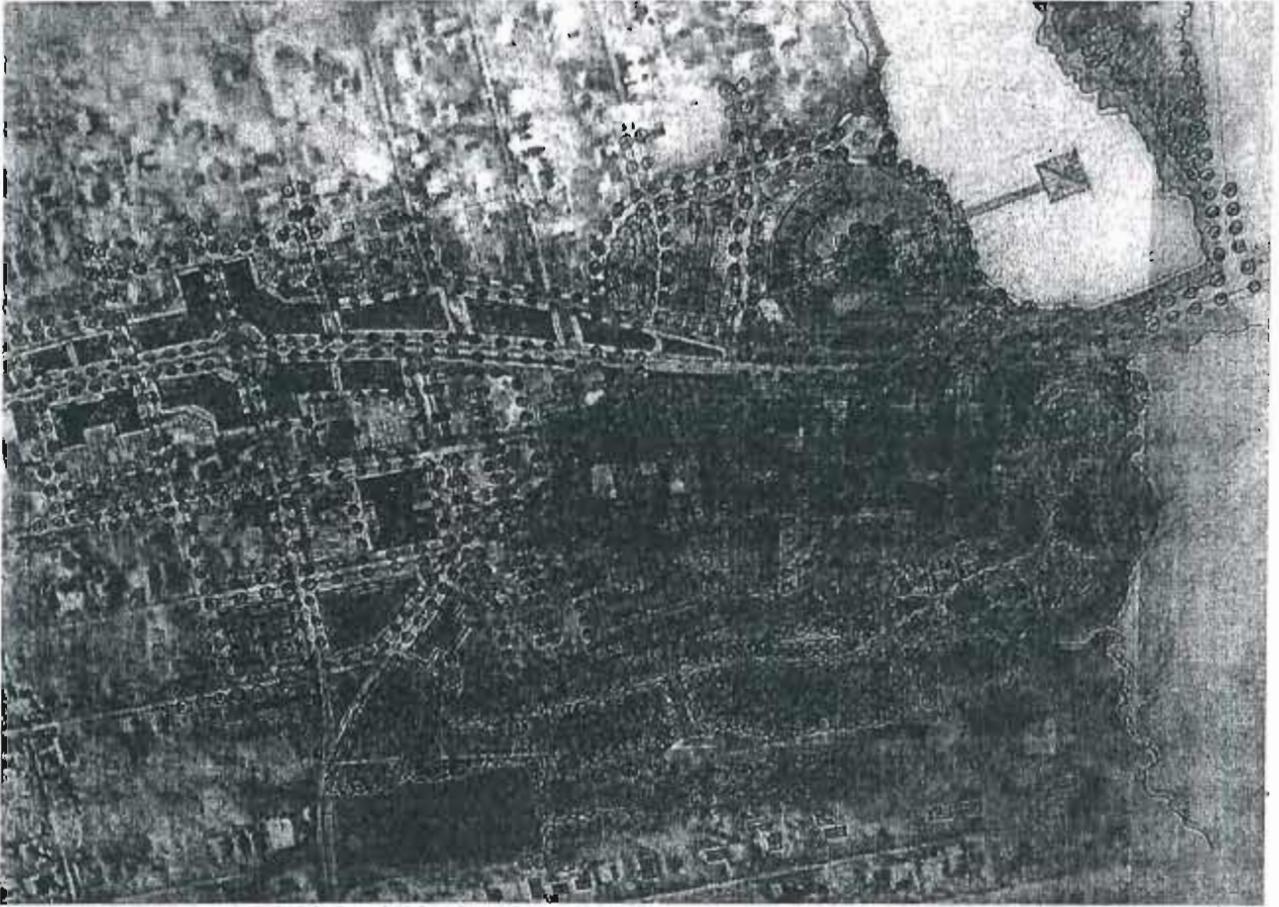
Two other major elements of developing this area as a second "Neighborhood" with a "downtown" or "main street" character include a civic building, in this case a library, and two parks, one a "green" in front of the library and the other park north of the library which would be more recreational in nature.



Well-designed civic buildings are lacking in the United States today and a well-designed Library can provide an enhanced level of civic pride. The library, centrally located along the 27A corridor can offer the children, adults and the elderly of the community a gathering place and the "lawns" or "green" in front of the Library and connected to the intersection gives this development a sense of a "village green".

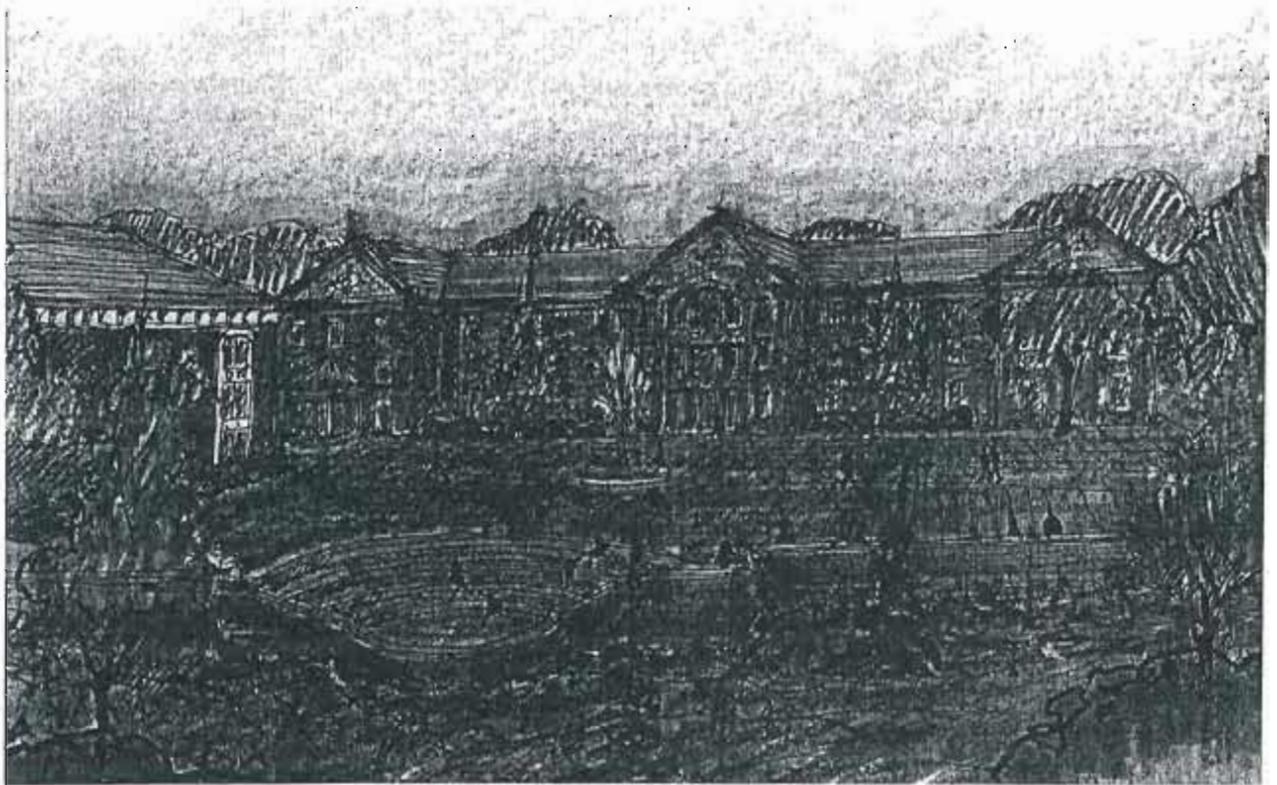


3. TWIN PONDS



The development of the third neighborhood center at Twin Ponds is centered around a proposed Cultural Arts Center.

Located on the southeast portion of this new neighborhood, the Cultural Arts complex commands beautiful views to the south and east of the existing wetlands and the water of Twin Ponds.

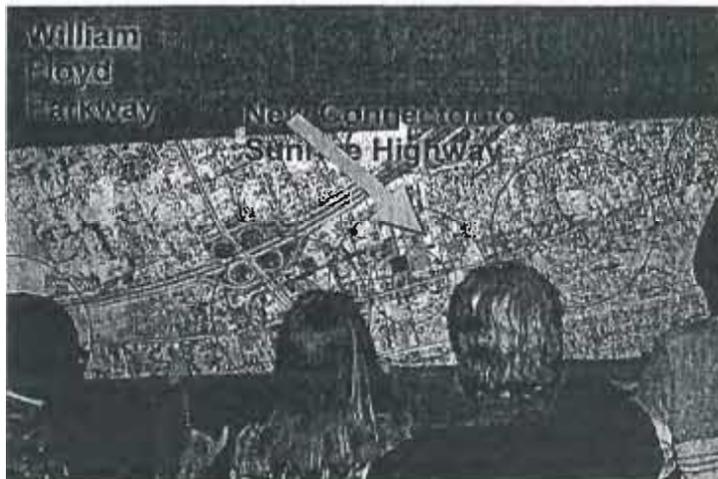


The center of the "Main Street" commercial area would be located at the intersection of Herkimer Street and 27A. Two story buildings fronting the streets with retail on the first floor and office and apartments on the second floor would be an indication of the building type in this downtown area. This commercial section of Twin Ponds would be the smallest of the three new neighborhood centers.

4. GENERAL RECOMMENDATIONS

A. Traffic Congestion: The following suggestions within the overall concepts of this phase will assist in relieving some of the traffic congestion evident today on the 27A corridor.

a. **Develop a south service road for Sunrise Highway**



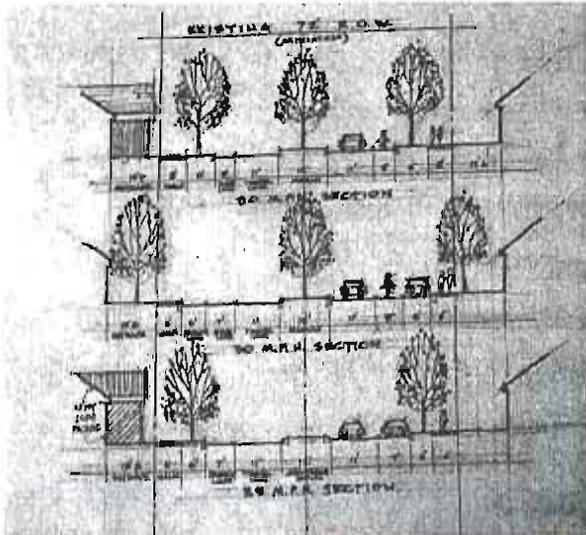
- b. Unblock the existing street grid and street fabric of the entire Mastic/Shirley area by building out existing paper streets or undeveloped streets.
- c. Develop new North/South travel access from Titmus Drive south to Hawthorne Street and enable Hawthorne Street to cross over or under the Long Island Railroad.
- d. Connect Barnes Road to the Sunrise Highway.
- e. Develop a variety of modes of transportation including walking, bicycle and bus.

In conjunction with unblocking the existing street grid by building out proper roads and “unclogging” the flow of traffic throughout the entire neighborhood north and south of the 27A corridor, some relief for the existing traffic congestion will be achieved.

Street Design Improvements

Streets serve many functions. They provide space for vehicles, trucks, bicyclists, and pedestrians to move along them and to cross them. Streets provide frontage for buildings/businesses and space for parked vehicles and delivery vehicles. They provide space for outdoor dining and drinking, space to sit and talk, space for trees, benches, street lighting, and street furniture. They provide space for street vendors, minstrels, and street dancers. But they are also the basic fabric of the transportation system. They provide mobility for pedestrians of all ages, bicyclists and vehicles and access to the abutting buildings.

Because streets provide for so many uses it is often difficult to provide a design that meets all of these needs. In the past when compromises had to be made they favored the automobile. Nowadays communities favor the design of streets that provide facilities for all users. As a consequence, space for vehicles is often reduced so that other important details such as trees, lighting, medians, wider sidewalks etc. can be added. Each of these elements builds upon one another to create a sense of place and add to the atmosphere of a village center.



Wider sidewalks, trees, and pedestrian grade lighting on both sides were added with benches. Bike lanes, medians and short refuge islands were utilized to help create an atmosphere of a town center. Roundabouts were added in place of traffic signals to reduce vehicle speeds, reduce delay caused by vehicle back-ups, and to improve the ease and safety of pedestrians across Montauk Highway.

Medians: Medians reduce delay, smooth out traffic speeds, help manage traffic at critical locations, channel conflicts to appropriate locations, provide spaces for landscaping, and provide a pedestrian refuge. This latter function helps pedestrians cross the road and reduces the number of pedestrian conflicts. The proposed medians will be 12 feet wide, providing space for pedestrians to share while crossing the road, and creating a safe, attractive, efficient tool for managing traffic. Median islands are generally 300 feet long. In some cases these medians can be shortened (called refuge islands) to as little as 60 feet.



As a general rule, medians and other access management tools reduce crashes by up to 50 percent on main roadways.

Turning Pockets: assist drivers by allowing autos to pull into a pocket out of mainstream traffic to await the opening of a gap. Motorists retain their left turn access in safer median turning pockets.

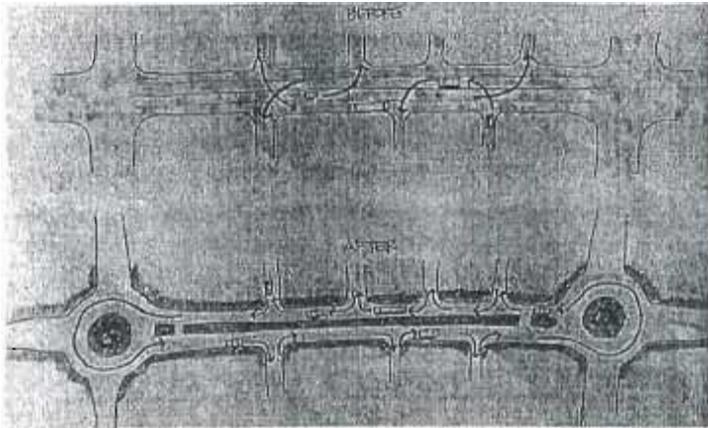
Roundabouts: Roundabouts are envisioned at several locations. Roundabouts were included, as explained above, to provide the necessary capacity, flow and speed moderation at critical points along Montauk Highway.

Modern roundabouts should not be confused with old-fashioned traffic circles. Roundabouts are much smaller, safer, and more efficient. Overall the roundabouts will reduce delay, decrease traffic back-ups and save the town of Brookhaven approximately \$3,000 to \$4,000 per year in operating and maintenance costs. In addition, the number of accidents will be reduced, the intersections will be prettier and pedestrians will find it easier and safer to cross at the roundabout.

In short, roundabouts are expected to:

- Improve the flow rate of traffic.
- Reduce crashes by 60-90%.
- Reduce vehicle emissions
- Reduce delay to all users of the intersection
- Improve appearance and aesthetics
- Improve access by allowing motorists to reach driveways they have just passed
- Improve walking conditions

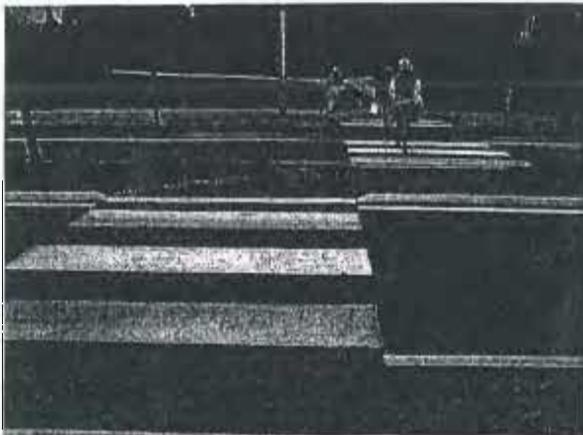




Pedestrians: Pedestrians need four conditions to cross a road safely:

- Low vehicle speed
- Simple decision making – deal with one direction of traffic at a time
- Adequate gaps between vehicles
- Visibility of conflicting vehicles

Pedestrians need ample width sidewalks on both sides of Montauk Highway. This study recommends 6' minimum width sidewalks on all sections, and 8-14 foot widths in key commercial areas, where right-of-way permits. Sidewalks are to be separated from the street with a planter strip whenever practicable. The minimum recommended separation is 4 feet. There are some physical constraints that reduce this sidewalk width.

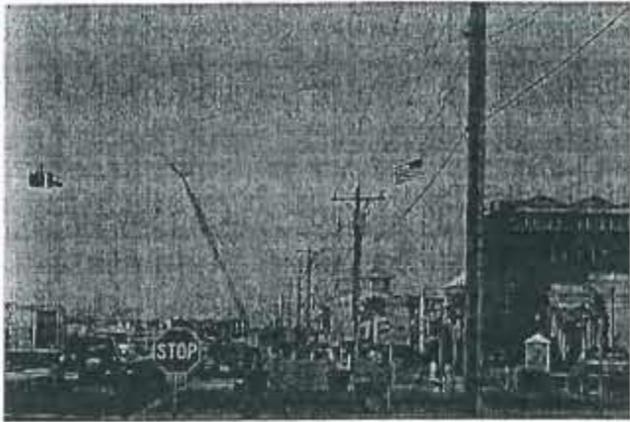


Crosswalks: Crosswalks should be provided at regular intervals along Montauk Highway. Pedestrians generally will not walk very far out of their way to use a crosswalk. As a general rule pedestrians will walk up to 150 feet to reach a well-defined, functional crosswalk.

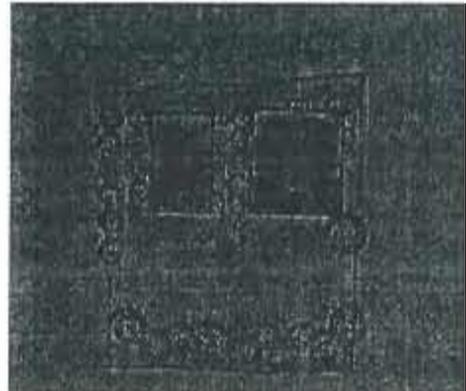
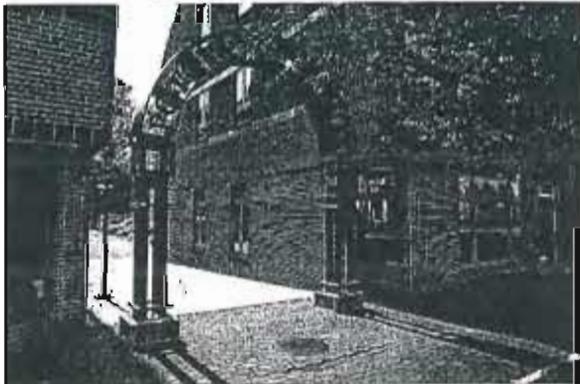


Bicyclists: Bicyclists will benefit from several changes. On Montauk Highway, bike lanes are recommended, serving as a five or six foot buffer between the parking lane or sidewalk and travel lane. The narrower appearance of the roadway will slow traffic speeds to about 30 mph. The added lane width can be found through improved lane markings. Travel lanes can be reduced to eleven feet each. Bike lanes add enhanced safety and operations to the road edge.

Underground Wires



Rear Parking



Part Four

Project Phasing And Implementation

Implementation

Principles of a Successful Community Plan

- Make plans with people, not for them
- Goals: Seeing the BIG picture
- Strategies: Getting from here to there
- Leadership: Committed and unified
- Governance: continue and open process
- Draw strength from your economic, cultural diversity
- Follow and find more money and resources
- Build and Sustain constructive partnerships
- Cultivate the media
- Be committed here for the long haul

A sustainable and livable community is created by making one improvement at a time. Sidewalks, street trees and other amenities are installed intersection-by-intersection, block-by-block. Parking lots can

become permeable and attractive, large lots can be screened and redefined by each owner. Public plazas and great blocks can start with one model. Individually, these changes do not create a community. Collectively, they provide the infrastructure needed for freshness, vitality, comfort, pride, welcome and a sense of place to work, live, play, shop and do business. In other words, the whole is greater than the sum of its parts.

Deliverables: It is very important to produce immediate results that stakeholders, the press and other people can see. For example, a new section of sidewalk, new street lights, benches and trash receptacles, drinking fountains or a freshly painted crosswalk are visible while a transportation plan is a paper document that may never be seen or appreciated by the public. The point is that a program, to keep its momentum, needs some quick wins. These early achievements are real and create the sense that something is happening; that government and community leaders are responsive.

Strategies: Improvements don't just happen. They require a well orchestrated thoughtful strategy that is implemented over time. The following strategies provide the structure under which Montauk Highway and its commercial shopping district can be realized.

Partnerships

William Floyd Community Summit and the Mastic Shirley Chamber of Commerce

In preparation for the Montauk Highway Project, a number of stakeholders with interests in common formed the Montauk Highway Project Committee. The partners have indicated a willingness to achieve a built-dream by working together. Continued development and collaboration of the Partnership is essential to realizing high quality results from the community vision.

The Montauk Highway Project currently consists of the following stakeholders:

- William Floyd Community Summit neighborhood and church leaders
- Town of Brookhaven, Suffolk County and Federal and State Government Partners
- Chamber of Commerce, Business and Property owners
- William Floyd School District, Library and other special districts

In order to be most effective, the Montauk Highway Project needs to be supported by local and regional agencies. Initial players should attract other followers and continue to take leadership in implementing short term and long term projects. Adding other stakeholders, such as significant businesses and employers inside and outside the direct study area and other neighborhood groups could enhance effectiveness of the effort. Significant changes to Montauk Highway and the surrounding area will come about as a result of continued development and collaboration among those in the Partnership. However, the mechanics of implementation may prove overly challenging to its volunteer nature. Therefore, the Partnership should consider creating a more formal organization to attend to the complexities of building the project.

Considerable opposition to change can be expected. The Partnership must be endorsed and backed by local government leaders. The Partnership must have the active participation of these leaders. Local government officials and elected officials must pledge their faith and pledge trust in the Partnership to work out necessary language and implementation steps for change.

The Partnership should resolve issues using fairness and effective communications. The Partnership should remain true to the community vision and handle appropriate change to accommodate neighborhood and business sensitivities and needs. These decisions must be driven by the vision, and not unsubstantiated fears.

Business Improvement District (BID) or Community Development Corporation

A business improvement district allows the Partnership to take control of its destiny and provides a single voice for promoting the interests of the area. The BID provides a means to generate its own revenue and allow it to set priorities for implementation. The Partnership acts as the board of directors of the business improvement district. The Business Improvement District or CDC could take on a number of functions including:

- Maintaining public spaces by, for example, pressure washing sidewalks, litter removal, snow removal, grooming the landscaping, and repairing damaged benches and trash receptacles.
- Installing and maintaining enhanced lighting
- Overseeing project development and design
- Matching capital funding from town, county, state, and federal funding partners
- Communicating with the community leaders and city/county officials including promotion and marketing of the Main Street District, providing information and resources for developers and real estate professionals.

The Partnership reviews state statutes and city/county ordinances to gain understanding of the authority to establish a Business Improvement District.

Funding the Vision

Reinventing Montauk Highway and the surrounding area requires multiple funding partners to tackle the varied components of the project. The Partnership identifies both conventional and non-conventional sources to supplement Business Improvement District/CDC revenues.

Conventional sources:

- Federal Surface Transportation Program Enhancement Funds for bicycle and pedestrian improvements, landscaping, and historic preservation
- Federal Transit Administration funding for transit facilities such as bus pull-outs, connections to the planned regional rail system, shuttle systems and park/ride lots
- Federal and State Highway Safety demonstration grants to address the high frequency of pedestrian hits in the corridor
- New York DOT and County highway and transit funds
- State appropriations for State capital improvements
- Town Property Tax allocated to capital projects
- Utility fees (for under grounding utilities)
- Sidewalk Improvement District
- Sidewalk Improvement Trust (Zero interest loan to retailers, pro-rated over 10 years)
- Building Façade Trust (Zero interest loan to retailers, pro-rated over 10 years)
- Tax Increment Financing



This attractive, well-loved pocket park in Langley, Washington has 17 design features unique to this park. Places that matter the most are built with passion, care, love and commitment. Money, materials and resources are found inside the neighborhood or business district. Projects such as this are built when a common community vision is achieved.

Non-conventional sources:

Non-conventional funding takes on many forms. Increasingly, public improvements are being realized through public/private partnerships. This type of partnership could take many forms such as community development corporations, neighborhood associations, "Friends" groups, private foundations, direct industry support and involvement of individual citizens. In fact, most public amenities, whether they are parks, children's playgrounds and fountains, street trees or the restoration of buildings, are the result of

individual people getting involved and deciding to make a difference. This involvement doesn't just happen, it needs to be encouraged and nourished by the Montauk Highway Partnership and other governmental authorities.

Access to most public funding sources can be enhanced by a willingness of private sector interests to invest in projects that benefit both public and private constituencies. Businesses could "adopt" portions of the street and take on responsibility for routine maintenance and minor physical upgrades. Key property owners could provide an annual contribution to implement the project and maintain public areas. The Partnership should engage the services of a well-respected public finance expert to explore the intricacies of special district financing.

Coordination with Utilities

The Montauk Highway corridor and the surrounding area house a variety of utilities including overhead electric, telephone, and cable lines as well as underground water, sewer, storm water, and gas lines. Others may include steam pipes, fiber optic lines, and computer network cables. Rapid expansion of the communications industry is placing an unprecedented demand on public rights of way.



"Piggybacking" pedestrian and other improvements on capital projects is one of the single best ways to make major improvements in a community. Sidewalks, curb ramps, landscaping, lighting and other amenities can be included in road projects, utility projects, and private construction in public rights-of-way (i.e. cable television, high-speed fiber optics etc.). To accomplish this, there are several things that can be done:

Utility companies can offer many ways of enhancing the roadway image. Improved canopy cover results from burying utilities, or raising them to a height that allows improved growth. Other coordination results in reduced costs or shared corridors for trails and improved links. (example: focus group session with utilities companies, Ft Meyers Beach)

- Contact all state and regional agencies, local public and private utilities that do work in public rights-of-way. Secure their five-year project lists as well as their long-range plans. Then, work with them to make sure that the streets are restored in the way that complements the vision for Montauk Highway.
- Look internally at all capital projects.
- Take advantage of every opportunity to make improvements at the time of construction.
- Consider combining small projects with larger capital projects as a way of saving money. Generally, bid prices drop as quantities increase.

Utility providers should be contacted to identify replacement needs and timing. Replacing utility lines usually includes many of the same components as road reconstruction such as demolition of pavement, excavation, and reconstruction of the roadway base. The Partnership should coordinate with utility providers with the goal of simultaneously replacing utilities and reconstructing Montauk Highway. That will allow cost sharing of common elements. It also defers the need for utilities to cut into new pavement and disrupt the street to replace utilities after the project is completed.

Projects of this magnitude usually require phasing to manage costs and cash flow as well as to minimize disruption to traffic and adjacent businesses. Part of the design work will include phasing and maintenance of traffic plans that, depending on revenue, will govern the time required to complete the project.

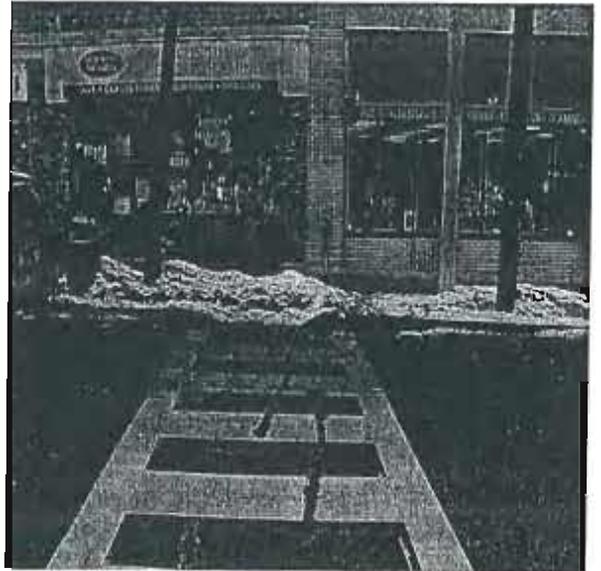
Comprehensive Plan and Development Code

The children and a number of older participants in the visioning process observed that the Town of Brookhaven should strengthen its code enforcement in the area. Stronger efforts to encourage repairing buildings, trimming landscaping, and generally sprucing up the area will enhance the image and help draw customers to the area.

Strengthening the development code would require developers and property owners to install or maintain public amenities such as sidewalks, street trees and curb ramps. Additionally, they would be required, through zoning, to locate buildings at the edge of the street with parking located behind buildings. The key is to focus on a few community priorities without creating hardships and disincentives for developers and property owners.

While the Town of Brookhaven can focus enforcement efforts on the area, perhaps the most effective way to give the area a facelift is through the volunteer efforts of businesses and property owners along the street. Organized efforts to periodically clean, pick up trash, and embellish the landscape builds pride in the area and improve its appearance.

Every effort should be made to develop and support the objectives of the comprehensive plan to encourage Mastic Shirley as a pedestrian-oriented urban village.



Many street environments greatly enhance their overall image and use with a simple to organize volunteer face-lift. Resultant pride of place helps kick-start significant investments.

Frozen snow banks blocking access, dumpsters, overgrown plants, litter, poorly maintained storefronts are common in many neglected Main Streets. Just two or three poorly maintained properties drag down the success of other healthy businesses. Suburban plazas and malls require minimal maintenance by leaseholders. Main Street requires the same level of care and attention.

Short-term Strategies

The Partnership should initiate and promote an annual program to make small, visible improvements. Examples include sidewalk replacement, community clean-up, annual tree planting, etc. This is an excellent way to create momentum and community support. Set a goal, for example, of adding 100 trees per year to key locations for a period of ten years. Since funds are limited, you will want to be very careful about the projects you select. There are several things to consider:

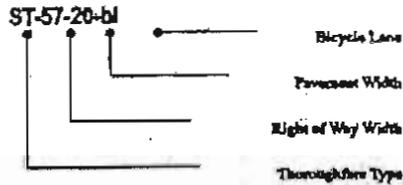
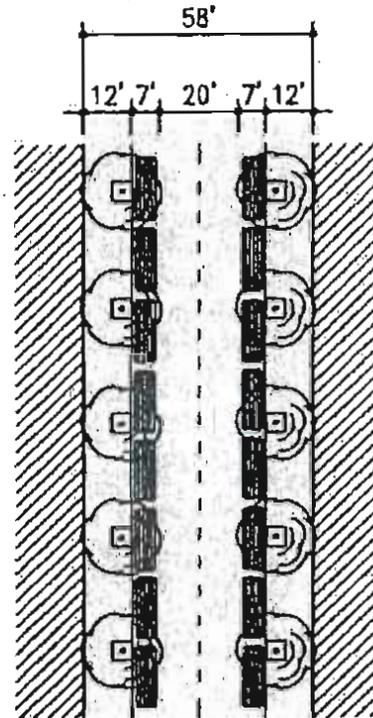
- Give priority to locations used by the greatest number of people, the elderly, those with disabilities and that provide access to transit. Gateway entries are another important place to focus efforts.
- Evaluate existing maintenance practices. Snow was seen as a constant threat to having attractive walkways, planter strips, medians and corner treatments. No town in America, despite high snow loads, should be held hostage to a lack of maintenance knowledge, equipment or practices.

Ongoing Care

Part of the lure of shopping experience resides in the consistent level of common area maintenance and predictable operation of facilities. These developments prosper from a coordinated effort to promote themselves through advertising and staging special events to draw customers. Montauk Highway should be viewed as a significant activity center. Area retail merchants and the Partnership will benefit from similar efforts to coordinate upkeep, promote its amenities, and to organize special events that will introduce new users to the area.



THOROUGHFARE TYPES	
PT:	Path
LA:	Lane
RD:	Road
ST:	Street
PS:	Passage
AL:	Alley
CS:	Commercial Street
AV:	Avenue
BY:	Boulevard
HW:	Highway
DR:	Drive
RR:	Rural Road
bc:	Bike Path (optional as noted)
bl:	Bike Lane (all, or a.s.)
lc:	Bike Lane (optional as noted)



Thoroughfare: the urban element that provides the major part of the public open space as well as paved lanes for vehicles. A thoroughfare is endowed with its attributes: capacity and character. Capacity is the number of vehicles that can move safely through a segment of thoroughfare within a given time period. It is physically manifested by the number of lanes and their width, by the centerline radius, the curb radius, and the supervision of the pavement. Character is the suitability of a thoroughfare as a setting for the desired transportation modes and as a location for a variety of building types. Character is physically manifested by the thoroughfare's associated building and signage types as determined by its location within the transit. The purpose each thoroughfare serves is described in descending order of travel priority.

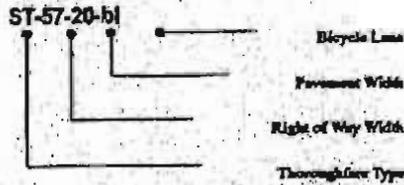
**LARGE COMMERCIAL STREET
CS-58-34**

Type
Movement
Design Speed and Posted Speed
R.O.W. Width
Pavement Width
Traffic Flow
Parking Layout
Curb Type
Curb Radius
Planter Width
Planter Type
Planting Pattern
Tree Type
Street Light Type
Street Light Spacing
Bike Way Type
Bike Way Width
Sidewalk
Sidewalk Width

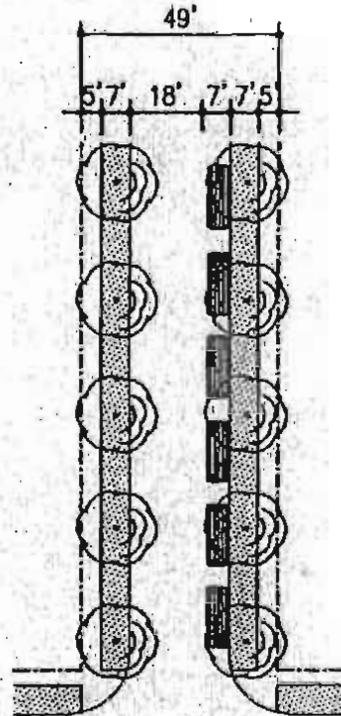
Commercial Street
Free
30
58'
34'
Two ways
7' both sides
Painted curb
14'
8' x 6' planters on both sides
Individual
Also 30' a.s.
T80
T80
35' o.c.
Bike route
N/A
Both sides
12'

THOROUGHFARE TYPES

- PT: Path
- LA: Lane
- RD: Road
- ST: Street
- PS: Passage
- AL: Alley
- CB: Commercial Street
- AV: Avenue
- BV: Boulevard
- HW: Highway
- DR: Drive
- RR: Rural Road
- bp: Bike Path (optional as noted)
- br: Bike Route (all M.U.A.)
- bl: Bike Lane (optional as noted)



Thoroughfare: the urban element that provides the major part of the public open space as well as paved lanes for vehicles. A thoroughfare is endowed with two attributes: capacity and character. Capacity is the number of vehicles that can move safely through a segment of thoroughfare within a given time period. It is physically manifested by the number of lanes and their width, by the centerline radius, the curb radius, and the super-elevation of the pavement. Character is the suitability of a thoroughfare as a setting for the desired transportation modes and as a location for a variety of building types. Character is physically manifested by the thoroughfare's associated building and signage types as determined by its location within the transect. The purposes each thoroughfare serves is described in descending order of travel priority.



RESIDENTIAL STREET / ROAD
ST-49-25 / RD-49-25

Type
Microroad
Design Speed and Posted Speed
R.O.W. Width
Pavement Width
Traffic Flow
Parking Lanes
Curb Type
Curb Radius
Planter Width
Planter Type
Planting Pattern
Tree Type
Street Light Type
Street Light Spacing
Bike Way Type
Bike Way Width
Microroad
Microroad Width

Residential Street/Road
Slow
25
60'
25'
Two ways
7' one side
Raised on street/Grade on road
16/25'
7' both sides
Continuous
Along 30' o.c. / Single and cluster avg 1/30'
TBD / Avg. 1/30'
TBD / None
One per intersection / None
Bike route
N/A
Both sides
5'

